



March 17<sup>th</sup>, 2022

JDE Project 21196

**Gerry Lemos**  
81 Mary Street  
Barrie, ON L4N 1T2

**RE: Parking Study**  
**81 Mary Street, City of Barrie**

This parking study was prepared by **JD Northcote Engineering Inc.** [JD Engineering] for the account of **Gerry Lemos** [Developer].

## **1.0 BACKGROUND**

The subject site, municipally known as 81 Mary Street, is located on the east side of Mary Street, north of Dunlop Street West.

The proposed development includes ground floor commercial space (71.41 sq.m.) and four residential apartment units. The proposed development will include 5 parking spaces, including one accessible Type A parking space.

Access to the subject site will be provided via a single full-movement driveway onto Mary Street [Site Access].

The Site Plan is provided in the **Appendix**.

## **2.0 STUDY OBJECTIVE**

The purpose of this study is to determine the parking demand for the proposed redevelopment and provide a recommendation for a parking strategy that will accommodate the anticipated parking demand.

## **3.0 CITY OF BARRIE BY-LAW PARKING REQUIREMENT**

The City of Barrie Zoning By-Law 2009-141 [ZBL] provides parking requirements for a variety of building types and land uses.

**Table 1** illustrates the parking requirement for the proposed development, according to the Zoning By-law.

**Table 1 – City Zoning By-Law Requirement**

Category	Requirements	Units	Parking Required (spaces)
Residential Dwellings (within Urban Growth Centre)	1.0 spaces per unit	4 units	4
Multiple use in a Commercial Zone	1 space per 24.0 sq.m.	71.41 sq.m.	3
<b>Total</b>			<b>7</b>
<i>Accessible Parking</i>	<i>1 space for developments requiring 5 – 25 spaces</i>		<i>1 (Type A)</i>

## 4.0 STUDY AREA

### 4.1 PARKING INFRASTRUCTURE

Metered parking is provided on the east side of Mary Street. The *City of Barrie's Parking Strategy Update* (IBI Group, March 2020) [City's Parking Strategy] notes a current provision of 21 parallel spaces between Dunlop Street W and Ross Street. On-street parking is prohibited on west side of Mary Street.

Parking is prohibited on City streets within the Downtown Business Improvement Area from December 1<sup>st</sup> to March 31<sup>st</sup> from 3:00 a.m. to 6:00 a.m.

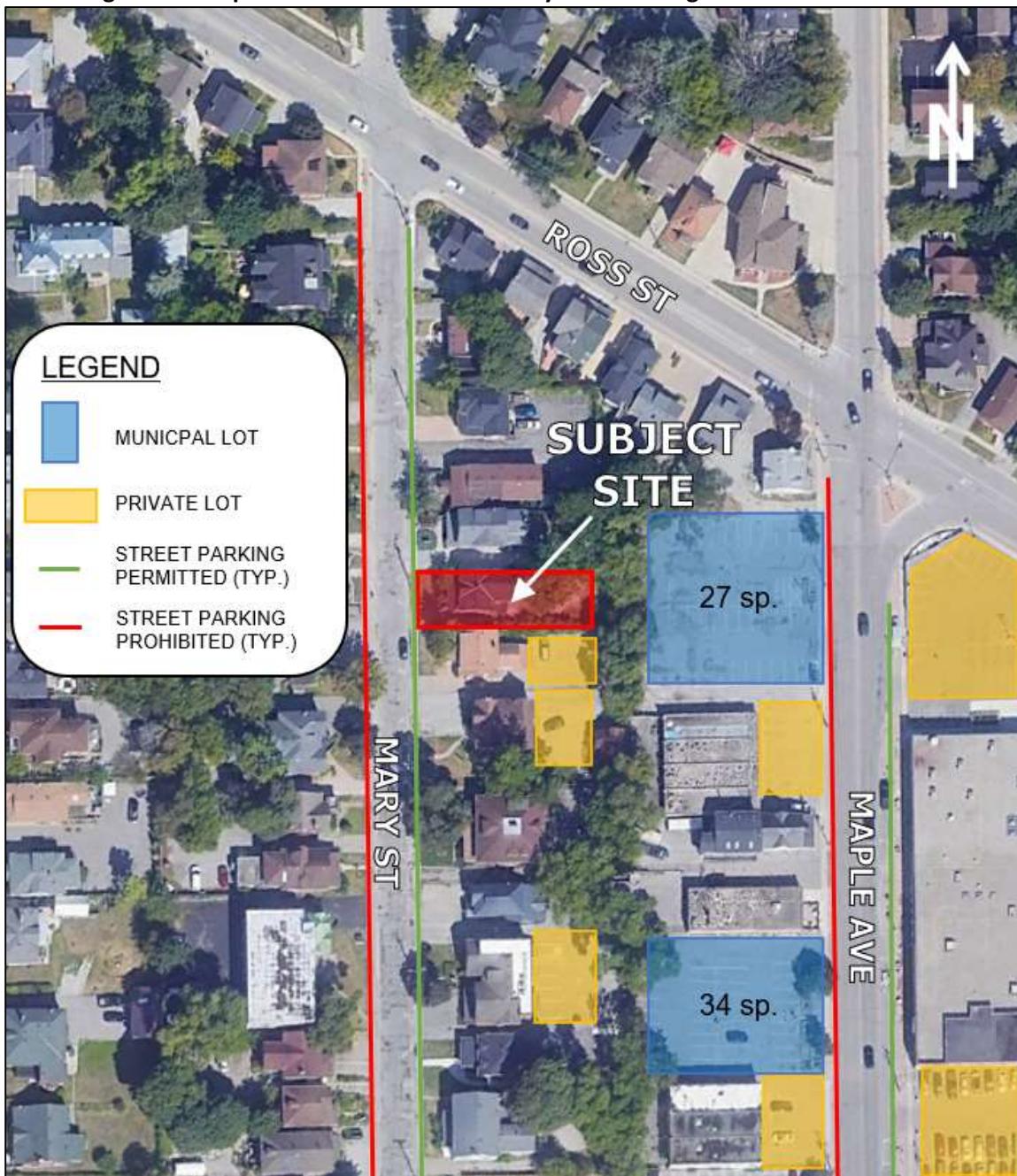
**Figure 1** illustrates the location of the above-noted existing parking in the study area.

In review of the City's Parking Strategy, the following parking utilization rates are projected for 2041 horizon year:

- Mary Street On-Street – 24%;
- Maple Avenue On-Street – 58%;
- Maple Avenue/Rose Street Lot – 81%;
- Maple Avenue Lot – 82%; and
- Collier Street Parkade – 90%.

The above parking capacities indicate that the residential visitors and commercial staff / clients of the proposed development will have access to municipal pay-and-display parking (both on-street and within larger lots) within the vicinity of the subject site, if additional parking is required.

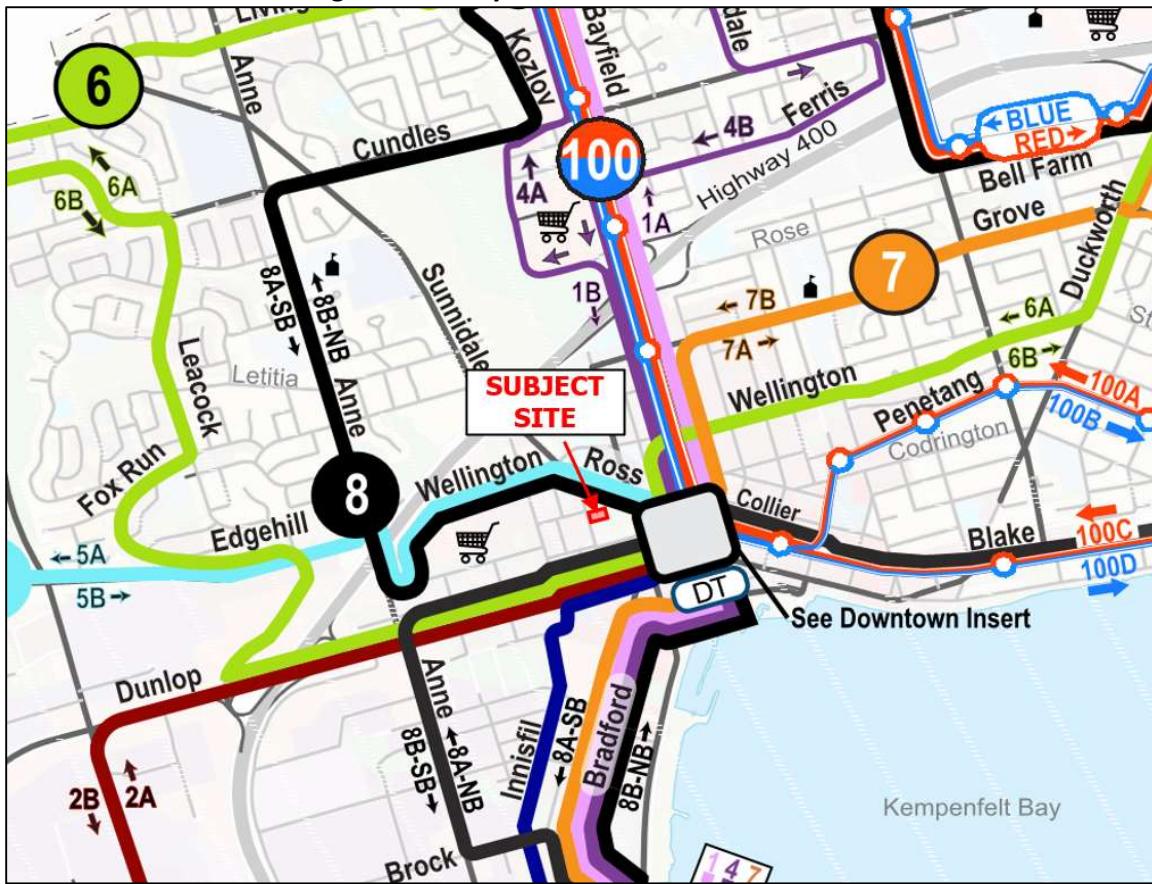
Figure 1 – Proposed Site Location and Study Area Parking Infrastructure



#### 4.2 TRANSIT INFRASTRUCTURE

Barrie Transit provides bus service within the study area. **Figure 2** illustrates the location of the subject site in relation to local transit routes.

## Figure 2 – Study Area Transit Routes



The subject site is located within 400 metres of the Barrie Bus Terminal, providing access to all nine transit routes with extensive service throughout the entire City.

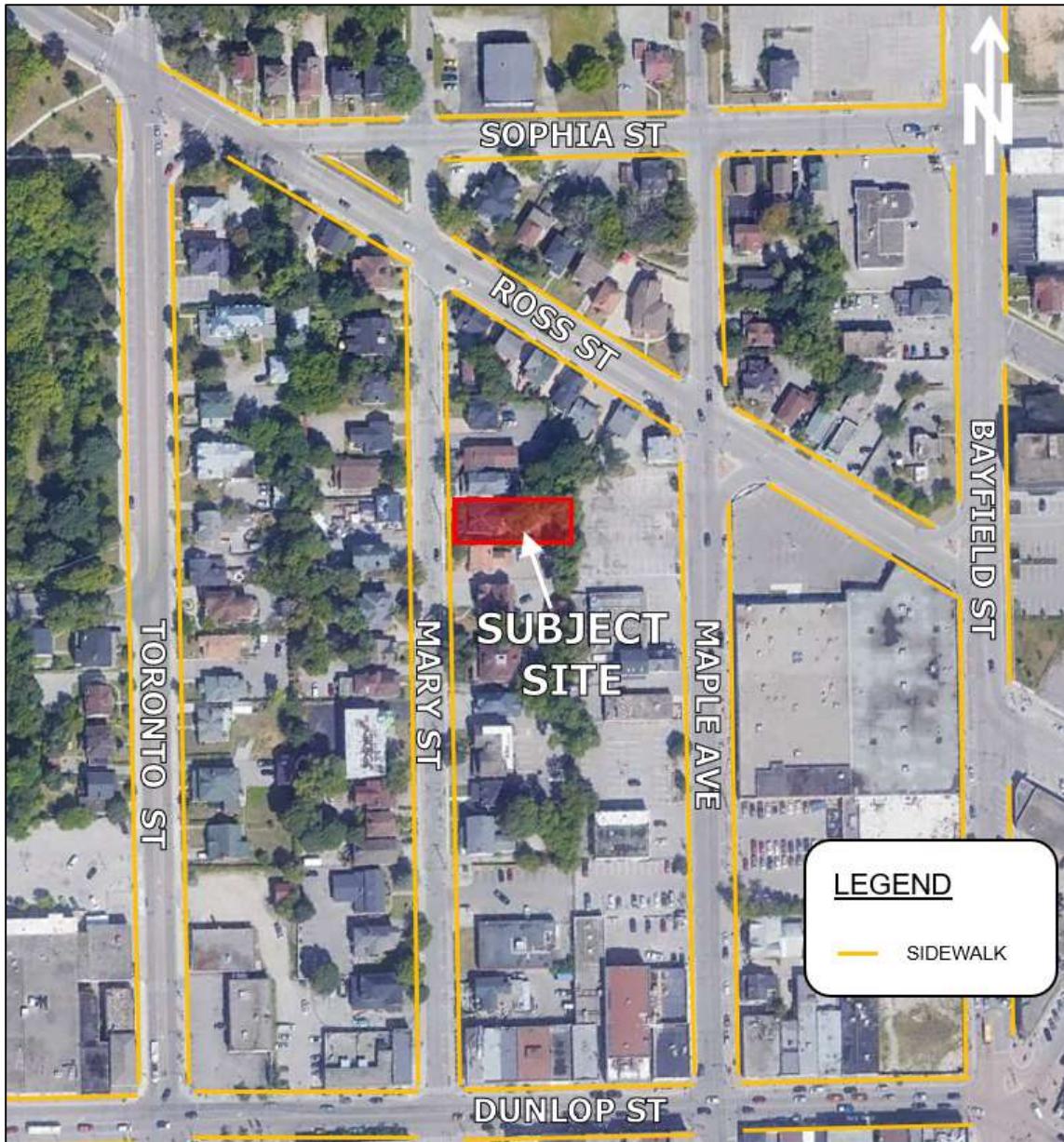
## 4.3 ACTIVE TRANSPORTATION INFRASTRUCTURE

The existing active transportation infrastructure within the study area is illustrated in **Figure 3**.

As illustrated, the subject site has direct access to the existing sidewalk on Mary Street, with continued connection through the downtown area.

Bicycle lanes are not available within the immediate area. However, the subject site is located within 500 metres of the Waterfront Multi-use Trail which provides access around Kempenfelt Bay and connection opportunities to the City's active infrastructure.

Figure 3 – Study Area Active Transportation Infrastructure



#### 4.4 LOCAL DEVELOPMENT AND AMENITIES

The Subject Site is located within a 700-metre radius of the following services and amenities:

- Convenience Store;
- Doctor, Dentist and various health care offices;
- Banks / Financial Institutions;
- Health Food Store;
- Pharmacy;

- City Parks and Playgrounds;
- Numerous take-out and dine-in restaurants (Tim Hortons, McDonald's, Subway etc.);

City has significant employment lands within the downtown area and to the north on Bayfield Street (via a short bus ride).

## 5.0 PARKING ANALYSIS

### 5.1 LOCAL AREA

The Subject Site is suited for a car-free lifestyle. The Subject Site is located within typical walking distance (700m) and short transit trip of many existing services and amenities and is also in close proximity to employment lands. The Subject Site is extensively serviced by public transit and active transportation infrastructure.

### 5.2 TRANSPORTATION DEMAND MANAGEMENT

The resident parking spaces for the subject site will be sold/rented separately from the units, which will provide a disincentive to private vehicle ownership and a basic level of Transportation Demand Management.

Perspective residents will be made aware that additional residential parking is not available at this site. The majority of the proposed development is not intended to service residents that require or are expected to require multiple parking spaces per unit.

In order to facilitate the reduced dependence on private vehicle travel, the following additional measures are recommended:

- Unbundled parking stalls from unit rentals (as noted above);
- Indoor resident bicycle parking supply (4 parking spaces);
- Outdoor resident visitor parking supply (2 parking spaces);
- Distribution of information packages to new residents, including transit and cycling maps; and
- Provision of an information display board in the residential common area of the building to share updated transportation information. The information board shall display travel information such as bicycle maps, local transit map/schedule, Smart Commute and other relevant information.

In summary, the residential units in this development are intended to service individuals or families that have one or fewer vehicles and businesses that have a limited staff and client parking demand.

### 5.3 VISITOR PARKING SUPPLY

The Zoning By-law does not identify a specific parking requirement for residential visitors. Residential visitor parking requirements for other similar municipalities were reviewed as part of our analysis. **Table 2** summarizes zoning by-law parking requirements for similar municipalities for residential visitors.

**Table 2 – Similar Municipalities Visitor Parking Requirements**

Municipality	Zoning By-law	Visitor Parking Requirements
Town of Newmarket	2010-40	0.15 spaces per unit
Town of Aurora	6000-17	0.30 spaces per unit
Town of Bradford West Gwillimbury	2010-050	0.25 spaces per unit
Town of Oakville	2009-189	0.20 spaces per unit
Town of Whitchurch-Stouffville	2010-001-ZO	0.25 spaces per unit

The visitor parking requirement identified in the above noted municipalities' Zoning By-Laws provide a reasonable reference for the Subject Site as the above noted municipalities have similar parking-related characteristics to the City of Barrie, including; population density, access to transit and access to active transportation infrastructure. Consequently, a visitor parking supply of 0.25 spaces / unit is recommended for the proposed development.

#### **5.4 RESIDENT PARKING SUPPLY**

In the case of visitor parking, when the visitor parking demand exceeds the visitor parking supply, the additional parking demand may result in unauthorized parking in nearby parking lots or undesignated areas. Consequently, providing adequate visitor parking supply, as outlined in Section 5.2, will ensure overflow visitor parking issues do not occur. Allocation of resident parking is different than visitor parking. Lowering the resident parking supply, in conjunction with clear communication during sales / rental process and ongoing parking enforcement, can increase development efficiency and provide a form of transportation demand management.

Historically, the cost to buy or rent an apartment or condominium unit has included one or more parking spaces, which provides an incentive for private vehicle ownership. As previously mentioned, the resident parking spaces for the subject site will be sold/rented separately from the units.

The Developer is committed to providing clear messaging in the sales agreement, which will inform buyers / renters that additional parking is not guaranteed and regular enforcement by the property manager will occur to ensure there is no misuse of parking within the site.

#### **5.5 COMMERCIAL PARKING SUPPLY**

The Subject Site will provide commercial ground floor area, noted as a 'personal services' use on the Site Plan. The parking demand for such is expected to be relatively minor in comparison with typical commercial uses (convenience store, retail spaces etc.). Employees and customers of the commercial use are proposed to utilize the available on-street or Parkade parking. Appropriate signage will be provided, notifying customers that on-site parking is not available.

#### **5.6 BICYCLE PARKING SUPPLY**

In order to facilitate a car-free lifestyle, resident and visitor bicycle parking spaces are required within the Subject Site. A total of 1.0 bicycle spaces per unit will accommodate the demand for resident bicycle parking and a total of 0.5 bicycle parking spaces per unit will accommodate the demand for residential visitor bike parking.

Bicycle parking is also recommended for the proposed commercial spaces. One short-term (visitor) bicycle parking space and one long-term (staff) bicycle parking space is recommended.

## 5.7 RECOMMENDATIONS AND PARKING ALLOCATION

**Table 3** illustrates the recommended parking supply for the Subject Site.

**Table 3 – Recommended Minimum Parking Breakdown**

Type of Parking	Recommended Parking Rate	Size	Recommended Parking Supply	Justification Criteria
Resident	0.75 spaces/unit	4 units	3 spaces	Control of Parking Spaces at Sale / Lease of Units
Visitor	0.25 spaces/unit		1 space	Proxy Municipalities
Commercial	-	71.41 sq.m.	-	Municipal Parking Utilization
Accessible	1 space for 5 – 25 required spaces		1 space	-
<b>Recommended Minimum Parking Supply</b>			<b>5 spaces</b>	
Bike (Resident)	1.0 spaces /unit		<i>4 spaces (long-term)</i>	
Bike (Visitor)	0.5 spaces /unit		<i>2 spaces (short-term)</i>	
Commercial			<i>2 spaces (1 short-term &amp; 1 long-term)</i>	

The proposed parking supply (5 spaces including 1 dedicated accessible spaces) is considered to be adequate for the proposed development. Overflow parking onto adjacent on-street parking or private parking lots is expected to be minimal.

With the above noted parking provision, the subject site parking will facilitate all 3 residential spaces, 1 visitor space and 1 accessible space. Commercial and additional visitor parking demands will be supported by municipal on-street and parking lot infrastructure.

## 6.0 CONCLUSION

### 6.1 SUMMARY

Oversupplying parking can result in induced parking demand, inefficient land use and an unequal allocation of the cost associated with private vehicle ownership. Providing excess parking supply within the Subject Site, would undermine the intent of this development.

The Subject Site is suited for a car-free lifestyle. The Subject Site is located within typical walking distance (700m) and short transit trip of many existing services and amenities and is also in close proximity to employment lands. The Subject Site is serviced by public transit and active transportation infrastructure.

The resident parking spaces for the subject site will be sold/rented separately from the units, which will provide a disincentive to private vehicle ownership and a basic level of Transportation Demand Management. Clear marketing communication and parking controls will ensure that renters are aware of that additional parking is not available and will not be made available in the future.

The subject site parking will facilitate all 3 residential spaces, 1 visitor space and 1 accessible space. Commercial and additional visitor parking demands will be supported by the municipal on-street and parking lot infrastructure.

A total of 1.0 bicycle spaces per unit is recommended for residents. A total of 0.5 bicycle parking spaces per unit is recommended for residential visitors. One short-term (commercial visitor) bicycle parking space and one long-term (commercial staff) bicycle parking space is recommended.

We trust that you find this letter satisfies your requirements.

Yours truly,  
**JD Northcote Engineering Inc.**



John Northcote, P.Eng.  
President

Any use which a third party makes of this report, or any reliance on or decisions to be made based on it, are the responsibility of such third parties. **JD Northcote Engineering Inc.** accept no responsibility for damages, if any, suffered by any third party as a result of decisions made or actions based on this project.

81 Mary Street  
City of Barrie

Parking Study

Date: 03/17/22  
Project No.: 21196

## Appendix

*KBK*

# 81 MARY STREET

BARRIE, ON

KBK ARCHITECTS INC.  
T: (1800)203-7010  
INFO@KBKARCHITECTS.CA  
SUITE 300, 25 SHEPPARD AVE. W. TORONTO

**SCOPE**  
1. DEMOLISH EXISTING DWELLING  
2. PROPOSED 3 STOREY 4PLEX BUILDING

## GENERAL NOTES

1. CONTRACTOR MUST VISIT SITE TO FAMILIARIZE WITH EXISTING CONDITIONS BEFORE QUOTING FOR THE JOB.
2. VERIFY ALL EXISTING MEMBER SIZES, ELEVATIONS, AND DIMENSIONS ON SITE PRIOR TO COMMENCING ANY WORKS.
3. REFER TO THE DRAWINGS FOR ALL DIMENSIONS AND CONDITIONS ON SITE.
4. CONTRACTOR TO SUBMIT DETAIL SHOP DRAWINGS PRIOR TO FABRICATION.
5. REFER TO NOTES AND SCHEDULES FOR SPECIFICATIONS & GENERAL NOTES.
6. DRAUGHTS MADE BASED ON SITE CONDITIONS. DESIGN ONLY, CONTRACTOR IS REQUIRED TO FOLLOW MEASURE AND DIMENSION LEVELS & CONDITIONS ON SITE.

DISCREPANCY BETWEEN STRUCTURAL & ON SITE CONDITIONS MUST BE BROUGHT TO THE ATTENTION OF ENGINEER & OWNER PRIOR TO COMMENCING ANY CONSTRUCTION AND FABRICATION.

7. THESE NOTES AND ALL NOTES ARE PART OF THE WHOLE APPLICATION.

CONTRACTOR(S) MUST THOUGHT NOTES AND TREAT ALL DRAWINGS AS PART OF ONE APPLICATION.

8. PRIOR TO THE REMOVAL OF ANY INTERIOR WALLS, VERIFY THE CONSTRUCTION AND SUPPORT REQUIREMENTS OF THE CEILING JOISTS OR BOTTOM CHORDS OF ROOF TRUSSES (ABOVE).

IF CEILING JOISTS ARE NOT SUPPORTED BY WALLS, METHOD AND DESIGN OF SUPPORT FOR THE CEILING AREA TO BE PROVIDED TO INSPECTOR ON SITE FOR APPROVAL.

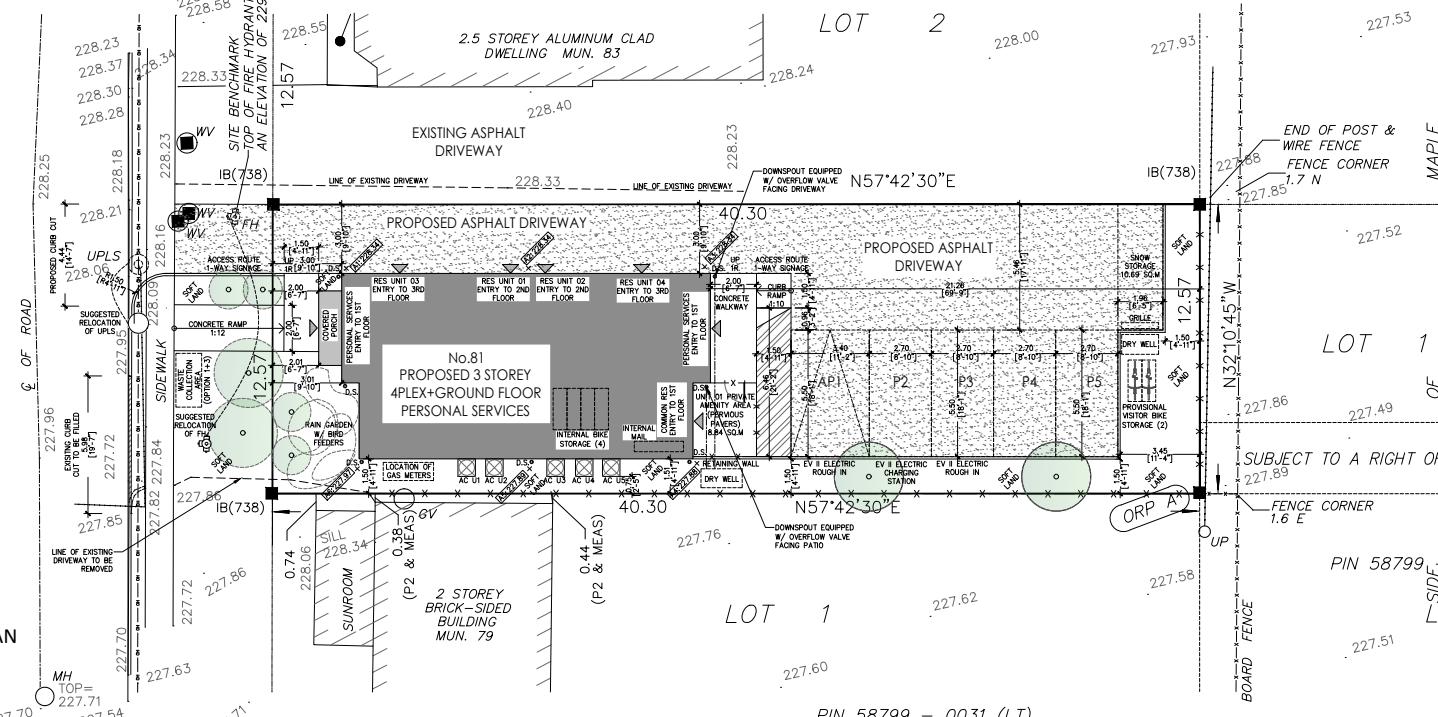
9. CONTRACTORS AND OWNERS REQUIRING ADDITIONAL INFORMATION OR CLARIFICATIONS ON THE DRAWINGS DURING CONSTRUCTION MUST CONTACT KBK ARCHITECTS INC BY EMAIL TO [INFO@KBKARCHITECTS.CA](mailto:INFO@KBKARCHITECTS.CA).

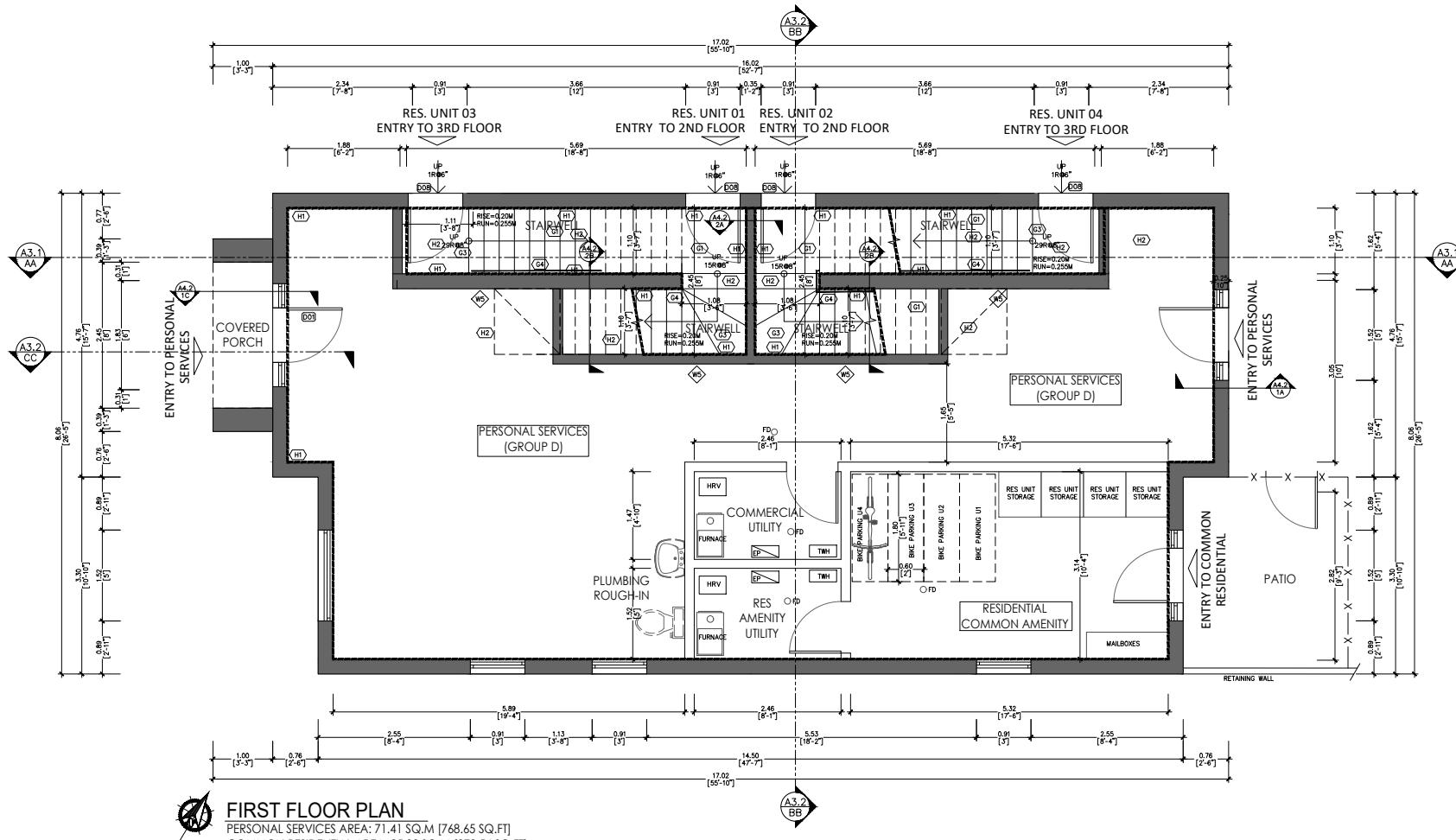
CONTRACTOR(S) AND CLIENT(S) ARE REQUESTED TO SEND A REQUEST FOR INFORMATION IN SUCH CASES, AND UNDERSTAND THAT THE REPLY TYPICALLY TAKES 48 HOURS. CLIENT(S) AND CONTRACTOR(S) ARE ASKED NOT TO MAKE PHONE CALLS WITH SUCH INQUIRIES TO AVOID MISCOMMUNICATION, AND TO HAVE ALL COMMUNICATIONS AS RFIs IN WRITING.



## KEY PLAN

MARY STREET  
(BY REGISTERED PLAN 17 AKA 121)  
PIN 58799 - 0021 (LT)





ABBREVIATION LEGEND

EX	EXISTING
PR	PROPOSED
F	FIXED
DW	DISHWASHER APPLIANCE
D	CLOTHES DRYER APPLIANCE
W	CLOTHES WASHER APPLIANCE
EP	ELECTRICAL PANEL
FD	FLOOR DRAIN
HRV	HEAT RECOVERY UNIT
HWT	HOT WATER TANK
DJ	DOUBLE JOIST
DO	DO-OVER (SAME AS OTHERS)
BP	BEAM POCKET
P/A	POST ABOVE
LVL	LAMINATED VENEER LUMBER
SPP	SPRUCE-PINE-FIR LUMBER
DF	DOUGLAS-FIR LUMBER
NS	NORTHERN SPECIES LUMBER

ALL DOORS STANDARD SWING UNLESS NOTED ON PLANS

EX SIGN CONNECTED ON EMERGENCY CIRCUIT  
 WITH EMERGENCY LIGHTS (10 W) COMPLY WITH  
 OBC 3.4.5

EMERGENCY LIGHTS EMERGENCY CIRCUIT  
 WITH (10 W) COMPLY  
 WITH OBC 3.4.5

SEE NOTE H1

INTERIOR DOOR LEGEND

DO1	36" DOOR, 8' HEIGHT	EXTERIOR, MAIN DOOR
DO2	36" DOOR, 8' HEIGHT	EXTERIOR DOOR
DO3	36" DOOR, 7' HEIGHT	INTERIOR DOOR, GLASS
DO4	36" DOOR, 7' HEIGHT	INTERIOR DOOR
DO5	32" DOOR, 7' HEIGHT	INTERIOR DOOR
DO6	30" DOOR, 7' HEIGHT	INTERIOR DOOR
DO7	18" DOOR, 7' HEIGHT	INTERIOR DOOR, BI-FOLD
DO8	36" DOOR, 8' HEIGHT	EXTERIOR DOOR WITH SELF CLOSER



KBK ARCHITECTS INC.

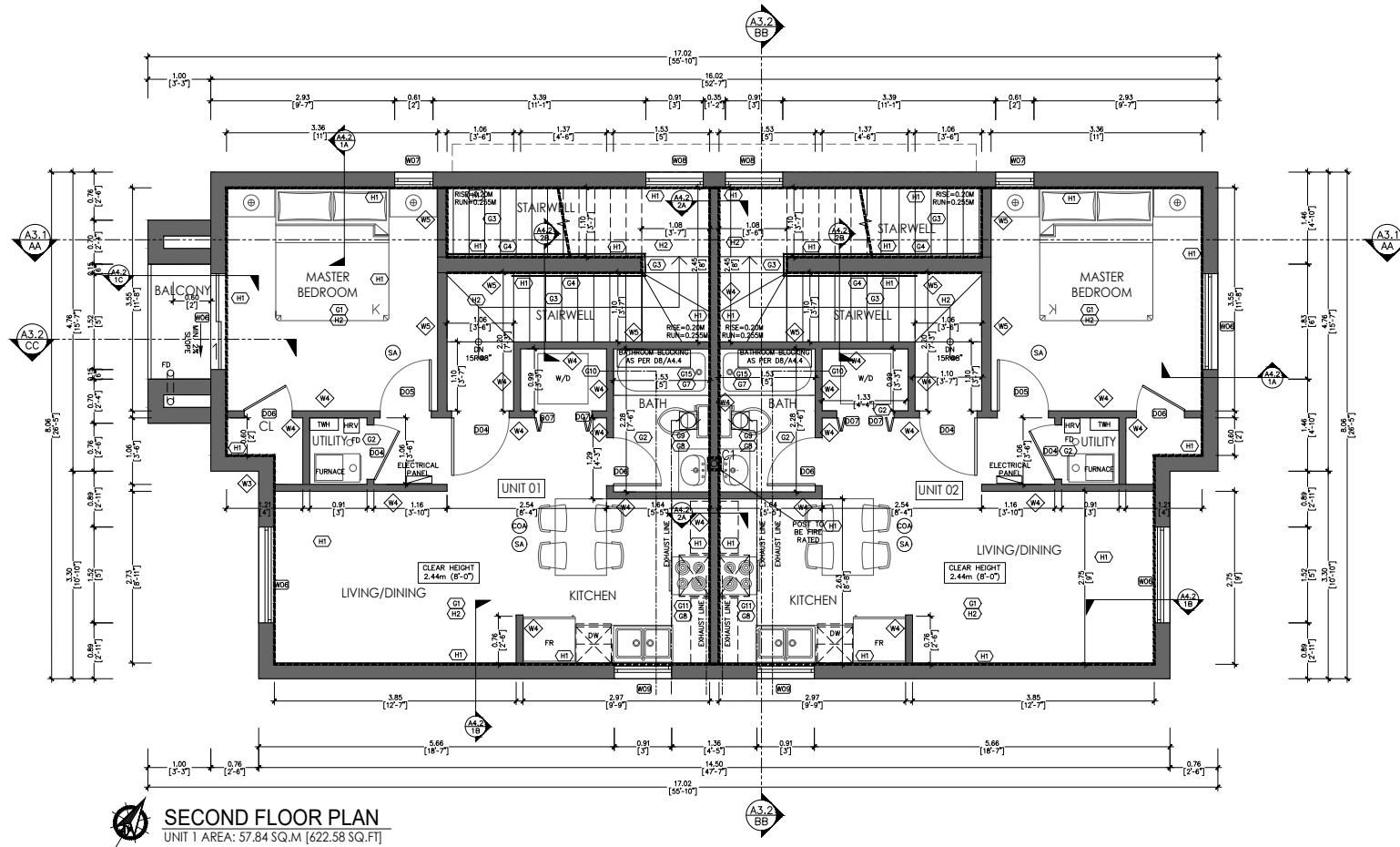
T: 1 (800) 203-7010  
 INFO@KBKARCHITECTS.CA  
 SUITE 300, 25 SHEPARD AVE. WEST  
 TORONTO M2N 6S4 ONTARIO

PROJECT ADDRESS  
 81 MARY STREET  
 BARRE, ON

DRAWING  
 FIRST FLOOR PLAN

DRAWN MINOR JL  
 PLOTTED DATE  
 1: 50 015x24"  
 DRAWN BY  
 KYLE KHADRA

A1.2



ABBREVIATION LEGEND

EX	EXISTING
PR	PROPOSED
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DW	DISHWASHER APPLIANCE
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DF	DOUGLAS-FIR LUMBER
NS	NORTHERN SPECIES LUMBER

ALL DOORS STANDARD SWING UNLESS NOTED ON PLANS

EXIT SIGN CONNECTED ON EMERGENCY CIRCUIT  
 EMERGENCY LIGHTS (10 Iu) COMPLY WITH  
 OBC 3.4.5

EMERGENCY LIGHTS, EMERGENCY CIRCUIT  
 WITH (10 Iu) COMPLY  
 WITH OBC 3.4.5

SEE NOTE 11

INTERIOR DOOR LEGEND

[001]	36" DOOR, 8' HEIGHT	EXTERIOR, MAIN DOOR
[002]	36" DOOR, 8' HEIGHT	EXTERIOR DOOR
[003]	36" DOOR, 7' HEIGHT	INTERIOR DOOR, GLASS
[004]	36" DOOR, 7' HEIGHT	INTERIOR DOOR
[005]	32" DOOR, 7' HEIGHT	INTERIOR DOOR
[006]	30" DOOR, 7' HEIGHT	INTERIOR DOOR
[007]	18" DOOR, 7' HEIGHT	INTERIOR DOOR, BI-FOLD
[008]	36" DOOR, 8' HEIGHT	EXTERIOR DOOR WITH SELF CLOSER

DESIGNER'S STAMP

GENERAL NOTES  
 ALL DRAWINGS ARE THE PROPERTY OF KBK ARCHITECTS INC. AND ARE NOT TO BE REPRODUCED IN WHOLE OR IN PART WITHOUT WRITTEN CONSENT FROM KBK ARCHITECTS INC.

CONTRACTOR TO CHECK AND VERIFY ALL DIMENSIONS BEFORE COMMENCING WORK. REPORT ANY DISCREPANCIES TO THE ENGINEER OR KBK ARCHITECTS.

ALL CONSTRUCTION TO BE ACCORDING TO THE CANON PRACTICE AND CONFORM TO THE ONTARIO BUILDING CODE.

NO. ISSUED FOR DATE  
 1. PRE-CONSULTATION FEB 19, 2021  
 2. SPA SUBMISSION OCT 29, 2021  
 3. SPA SUBMISSION DEC 07, 2021

KBK ARCHITECTS INC.

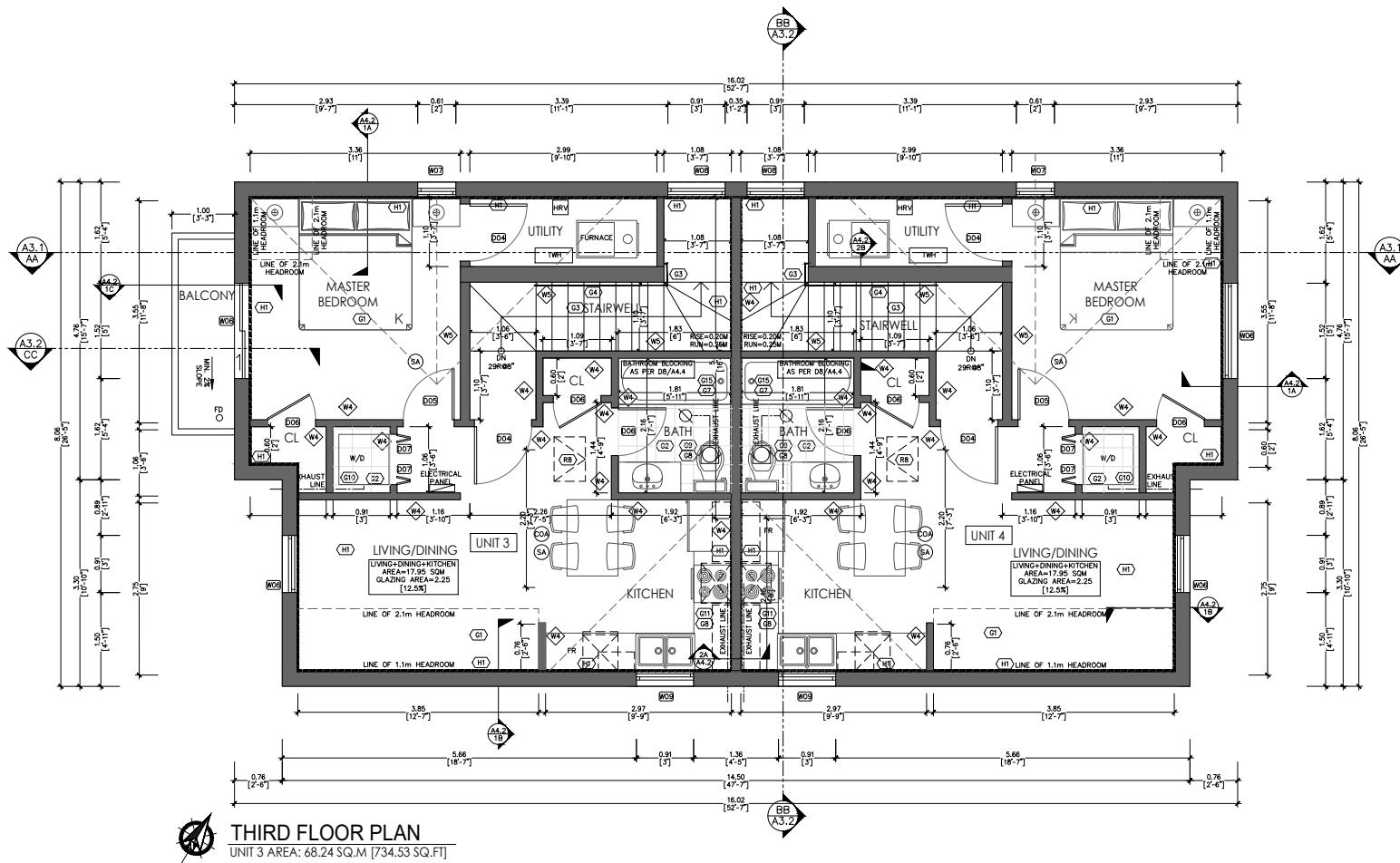
T: 1 (800) 203-7010  
 INFO@KBKARCHITECTS.CA  
 Suite 300, 25 Sheppard Ave. West  
 Toronto M2N 6S4, Ontario

PROJECT ADDRESS  
 81 MARY STREET  
 BARRE, ON

DRAWING  
 SECOND FLOOR PLAN

DRAWN MINOR JL  
 PLOTTED DATE  
 DEC 07, 2021  
 SCALE  
 1 : 50 @18"X24"  
 DRAWN BY  
 KYLE KHADRA

A1.3



THIRD FLOOR PLAN

UNIT 3 AREA: 68.24 SQ.M [734.53 SQ.FT]  
 UNIT 4 AREA: 68.24 SQ.M [734.53 SQ.FT]

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DRAWING THIRD FLOOR PLAN

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 SCALE 1: 50 @18"X24"  
 CHECKED BY KYLE KHADRA

A1.4