



# INNOVATIVE PLANNING SOLUTIONS

planners • project managers • land development

June 1, 2020

City of Barrie Ontario  
70 Collier Street  
L4M 4T5

Attention: Carlissa McLaren, RPP  
Senior Planner, Planning and Building Services Department

Re: Planning Justification Addendum (revised from 2019)  
Zoning By-law Amendment and Draft Plan of Subdivision Applications  
339 Veterans Drive & 341 Veterans Lanes  
City of Barrie

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## 1.0 INTRODUCTION

This addendum has been prepared in relation to the revised submission for Zoning Bylaw Amendment (“ZBA”) and Draft Plan of Subdivision (“Draft Plan”) applications for lands legally described as Lot 1, R.P. 1200, and Part 2, Reference Plan 51R-22069, known municipally as 339 Veterans Drive & 341 Veterans Lane. The original submission was made October 9, 2019. This addendum and submission should be reviewed in conjunction with the original submission.

The landholdings are made up of 339 Veterans Drive and 341 Veterans Lane, the Veteran’s Lane (municipal) ROW (“ROW”) and a parcel of land along Monsterrand Street (Roll No. 434204001801060). See **Figure 1** below. The entire landholdings represent an area of approximately 0.927 hectares (2.30 acres), with 70.18m of frontage along Monsterand and 118.3m along Veterans Drive. The purpose of the ZBA is to obtain the necessary zoning approval for a 78 unit mixed residential ‘block/cluster’ townhouse and apartment development.

The proposed ZBA aims to rezone the lands to Mixed Use Corridor with Special Provisions (MU2-SPXX). Further details of the development and associated applications are provided under **Section 2** of this report.

## 2.0 DESCRIPTION OF DEVELOPMENT

This Section will provide a detailed description of the revised development concept and the special zoning provisions required to permit the development to proceed.

**Figure 1 – Subject Lands**  
Source: Simcoe Maps



## 2.1 DEVELOPMENT CONCEPT

Through ongoing consultation with City Staff including Planning, Fire, Traffic and Engineering Departments, as well as comments received from the Neighbourhood and Public Meeting from both Council and members of the public, a series of revisions have been made.

The revised concept utilizes four parcels, as noted above, providing a larger landholding and eliminating Veterans Lane as a municipal road. Closing Veterans Lane was the major concern raised during the neighbourhood meeting, public meeting and throughout all discussions with City Staff. City Staff have urged the closure and recommendation to include these lands within a revised design concept. In addition, a small parcel that fronts Montserrat Street, which is located between the subject lands and the first dwelling on the south side of Montserrat Street, has been purchased by the applicant as per Staff's request to consider including these lands in the proposed development.

The entrance to the project continues to be accessed off Montserrat Street, and provides a right in entrance, left out only exit. This will restrict traffic from travelling down Montserrat Street into the adjacent subdivision, which has been an ongoing concern of the neighbouring residence. The proposed width of the two travel lane condo road is 6.0 metres, in keeping with OBC Fire Code standard and corresponds to road widths approved and required by guidelines

in urban areas such as the City of Toronto (see provided City of Toronto Road Design Guidelines, **Appendix B**). The evolved width of the road continues to build on the urban design guidelines and principles and the walk ability of the site, and serves to slow vehicular traffic, creating a more livable environment and increase pedestrian safety. The road will continue to operate and function as required while providing the necessary fire route, with a turnaround stub road between the apartment building and the stacked townhouses, seen as appropriate given the development proposal's need to have a strong streetscape along Veterans but also allow for an entrance to the building, accessible nearby parking and distance for balconies between the Part 3 OBC building forms. A landscape buffer has been provided along the entrance along the east lot line, adjacent to the dwelling on Monsterrand Street. The five storey building transitions to four storey stacked townhouses, providing a consistent urban frontage for the project with appropriate setbacks and scale, as per City intensification policies.

The project has been modified from a small walk up apartment and townhouses to the following, which accounts for the increase in units due to the larger apartment, while other unit types are decreased in numbers. The proposed concept (**Appendix A**) would introduce 78 residential units, with the unit breakdown is as follows:

- Townhouses, 3 storeys (Type A and Type B) – 19 units (Part 9 OBC)
- Stacked Townhouses, 4 storeys (Type C) – 20 units (Park 3 OBC)
- Apartment, 5 storeys (Type D) – 39 units (Part 3 OBC)

The apartment building is appropriately located to create a strong base for the community across from the commercial plaza at the corner of Veterans Drive and Monsterrand Street. The building is proposed to be 5 storeys with 39 dwelling units and structured parking. Affordable housing units will be accommodated in the building as required and will be accessible as an elevator will be provided. All dwelling units are planned to have a private outdoor balcony for amenity space.

The building has been brought forward to Veterans Lane to provide a strong street presence, per the urban design guidelines. The majority of the parking will be provided underground, along with the garbage room. Underground parking will have a few accessible parking spaces and all other underground spots will have an option to add a stacker to create two parking spaces per dwelling. The remaining required parking is provided at grade, to the south of the building, directly adjacent the building entrance. This parking area will be landscaped and screened from view along Veterans Drive.

The fire route, as indicated on the concept plan, is along the entrance condo road. A 6 metre fire route is provided with roll curb on the east side and depressed curb towards the stacked townhouses. This road cross section has been discussed, at length, with the Fire Department. The fire route would end at the south end of the site, and not travel along the one-way Woonref. The 6 metre width aligns with the OBC Fire Code. Hydrants would be placed along the route. Fire has indicated that the three buildings facing Veterans Drive would be accessed from that municipal right of way, and that the town house blocks inside would be accessed from the fire route, and trucks at no time needing to travel on the Woonref.

Travelling south along the condo road there are two blocks of stacked townhouses, providing a total of 20 dwelling units. These blocks will be four stories in height. Similar to the apartment building, these blocks have been pushed towards Veterans Drive, with front doors and walkways where required, facing Veterans. Again, this aligns with the urban design guidelines,

providing built form along the street, framing the road, contributing to density. These dwelling units will each have two parking spaces, in tandem. In addition, over outside the parking space, rear decks/balconies will be provided, with some front and side facing as per OBC requirements, adding private outdoor space to each unit.

As the condo road approaches the south most part of the lands, a one-way Woonerf provides access to Blocks 1, 2 and 3, with a total of 16 townhouse dwelling units along the east and north of the property. These townhouses will be three storeys in height, and each have three parking spaces, provided side by side, with an optional third space on the 2.7 metre parking lane along the Woonerf or on driveways as per plans. The 2.7m wide parking lane also allows visibility when entering/leaving the garages and a more than adequate turning radius for vehicles to enter the travel lane safely. This will also allow delivery vehicles to temporarily park on the parking space of a house and not on the travel lane.

The design of the 3.0-metre-wide Woonerf is to slow traffic to 10km/hr and allow people to use the area for recreational use. The one-way direction going north is designed to push vehicles to the outside eastern edge, adjacent to a 3-metre landscape buffer for trees and LID. The Woonerf provides sufficient access to the double wide garages for the dwellings in Blocks 1, 2 and 3. Very large planting landscape ornamental pots will be interspersed along the Woonerf to provide additional landscaping elements. Ground landscaping has not been successful in similar projects in the past, therefore the vegetation is to be consolidated on the eastern buffer lands.

The standard townhouse dwellings will be net zero ready, and the stacked townhouses will be part of the CHBA Net Zero MURB pilot program.

A large park, over 646m<sup>2</sup>, is provided in a central area of the site. The programming of this land will be reviewed through detailed design.

Snow storage can be accommodated around the site or it will be trucked off site if required, and heated roads are being considered at site plan stage.

Visitor parking is provided in a number of locations throughout the site.

The 2.75 metre road widening along Veterans Drive will accommodate a future multi-modal path (MMP) along with landscaping. After reviewing the details regarding the road widening, it was determined that the width varies along the property line and the project is asking for some consideration to allow for the two car parking of the stacked townhomes, as City staff requested the homes be brought forward to Veterans Drive and as the widening is for a MMP and not further travel lanes. The road widening has been measured from centreline of road and is indicated on the concept plan. The apartment and stacked townhouses along Veterans Drive frontage will connect to this path and have been placed so as to allow for more hardscaping of the area, adjacent to the future wide MMP.

A daylighting triangle, 5m by 5m, has been provided at the corner of Veterans Drive and Monsterrand Street. This daylighting triangle allows the apartment building to be pushed forward to the street and intersection as requested by Staff. In addition, as noted above the road widening if for a future multi-modal path and therefore sight lines will be maintained.

## 2.2 DRAFT PLAN OF SUBDIVISION

The first submission included a Draft Plan of Subdivision application. Based on the revised site plan design and further discussions with the retained surveyor, it is proposed that a draft plan of subdivision application is no longer required to facilitate the proposed development. Please refer to the Memorandum from Krcmar Surveyors Ltd. and the Site Plan with the draft plan of subdivision overlay, provided under **Appendix C**.

## 2.3 ZONING BY-LAW AMENDMENT

The subject lands are currently zoned 'R1' and 'RH'. To facilitate the proposed 78 unit townhouse & walk-up development, a site-specific ZBA to rezone the lands as 'Mixed Use Corridor with Special Provisions (MU2-SPXX)' is required; a copy of the draft Zoning Bylaw and Schedule A is provided under **Appendix D**. The ZBA would allow the lands to be developed as per the conceptual site plan.

**Table 1.** Mixed Use Corridor with Special Provisions (MU2SPXX) Zoning Matrix

<b>Mixed Use Corridor (MU2)</b>			
<b>Provision</b>		<b>Required</b>	<b>Provided</b>
<b>Lot Area (min.)</b>		-	8,944.60m <sup>2</sup>
<b>Lot Frontage (min.)</b>		-	70.18m Montserrand St
<b>Front (min.)</b>	<b>Yard Residential use abutting lot frontage</b>	3.0m	4.1m Montserrand St
<b>Side Yard</b>	<b>Min.</b>	-	1.8m
	<b>Min. abutting street or laneway</b>	3.0m	0.5m Veterans Dr.
	<b>Max.</b>	-	-
<b>Rear (min.)</b>	<b>Yard Abutting Street or Laneway</b>	1.5m	n/a
	<b>Abutting Residential, OS, or EP Zone</b>	7.0m	5.0m
<b>Front Façade Step-back (min.)</b>		45 degree angular plane at height above 80% equivalent right-of-way using 3m minimum step-backs; therefore, since Montserrand is considered to be the front lot line/yard, a step-back is required at 16m height	Not required

<b>Side Façade Step-back (min.)</b>		If adjacent to an OS zone, 5.5m at height above 80% equivalent of right-of-way	N/A
<b>Rear Façade Step-back (min)</b>		45 degree angular plan above 7.5m using minimum 3m step-backs	N/A
<b>Lot Coverage (max. % of lot area)</b>		-	50%
<b>Gross Floor Area (max. % of lot area)</b>		-	135%
<b>Street Level Floor Height (min.)</b>		4.5m	3.0m
<b>Building Height</b>	<b>Min.</b>	7.5m	7.5m
	<b>Max.</b>	16.5m	Towns – 11m Stacked towns – 14.5m Apartment – 20m
<b>Dwelling unit floor area (min.)</b>		1 Bed ..... 45m <sup>2</sup> 2 Bed ..... 55m <sup>2</sup>	1 Bed ..... 45m <sup>2</sup> (min.) 2 Bed ..... 55m <sup>2</sup> (min.)
<b>Outdoor amenity area (min.) (inclusive of landscape open space, rooftop amenities, private balconies or other outdoor amenity features, in a consolidated or unconsolidated form)</b>		12m <sup>2</sup> / unit	>12m <sup>2</sup> / unit unconsolidated
<b>Landscape Buffer Areas – where any lot in MU Zone abuts a lot in a Residential Zone</b>		3.0m	0.7m
<b>Parking Spaces (min.)</b>		1 spaces / unit	147 spaces (> 1 spaces / unit)
<b>Barrier Free Parking Spaces (min.)</b>		4 BF; 2 Type A and 2 Type B	4 BF; 2 Type A and 2 Type B
<b>Tandem Permitted</b>		No	Yes
<b>Parking for Apartment Dwellings (5.4.4.1)</b>		35% max lot coverage	<35%
<b>Driveway Length (5.2.5.2 d)) (min.)</b>		6.0m	5.5m
<b>Accessory Buildings and Structures Decks</b>	<b>Height (max.)</b>	4m	<4m
	<b>Occupy any part of a front yard</b>	Not permitted	Permitted. Decks/balconies occupy the front yard (Monsterrand St)

	<b>Occupy any part of a side yard</b>	Not permitted	Permitted. Deck/balconies occupy the side yard (Veterans Dr.)
	<b>Rear lot line setback (min)</b>	0.6m	>0.6m
	<b>Side lot line setback (min)</b>	0.6m	0.0m to Veterans Dr
	<b>Front lot line setback (min)</b>	n/a	0.0m to Monsterrand St

The proposed MU2-SPXX zone would permit the special provisions noted in **Table 1**. The special provisions requested are not anticipated to have adverse impact on existing and adjacent residential uses, or future residents of the proposed development. The proposed zoning provides for greater flexibility. The site details will be refined through the Site Plan process. Below is a detailed summary of the Special Provisions requested, as well as associated planning rationale:

**Reduced minimum side yard setback, abutting a street or laneway – from 3.0m to 0.5m (to Veterans Drive)**

Rationale: The lot line with frontage along Veterans Dr. has been interpreted as an exterior side yard, however its frontage and the orientation of the built form make it act similar to a front lot line/front yard. The required minimum side yard setback abutting a street or laneway is 3.0m, 0.5m is proposed. It should be noted that the 0.5 metre setback only pertains to the 5 storey apartment building and its setback to the sight triangle. The stacked townhouses are all proposed at a minimum 1.5 metres from this interior side yard.

The intent of this setback is to ensure a suitable distance is provided between the municipal ROW and proposed structure. The block/cluster stacked townhouses and apartment building have been pushed to the street to align with the municipal urban design guidelines in that they will be designed to complement and contribute to a desirable community character in terms of massing and design as experienced from the street. Interesting glazing options, materials and architectural styles will be considered on all facades oriented toward the municipal ROW. Any entrance with frontage on a municipal ROW will also be emphasized through materials design, and in part, proximity to municipal sidewalks and the future MMP.

A 0.5 metre setback proposed is considered suitable in this location. Dwellings along this frontage will be connected to the street through integrated sidewalks and landscaping, enforcing the urban design connection. The multi-modal path and boulevard will provide planted and landscape areas between the street and dwellings will also be refined through the site plan process. The proposed reduced side yard setback will maintain a suitable setback from the municipal road adjacent and would maintain the intent of the provision. Based on the above, the proposed reduced exterior yard setback is considered appropriate.



**Reduced minimum rear yard setback, abutting Residential, OS or EP Zone – from 7.0m to 5.0m**

Rationale: The proposed 5.0m rear yard represents a reduction of 28.5% from the standard MU2 zone provision. This is assuming that the south east corner of the property is considered the rear yard (City Staff to verify). This reduction is not anticipated to be significant enough to impact the functionality of site, or the private rear yards of Block 4, or the privacy of the adjacent lot they back onto. Significant landscaping can further address privacy concerns and will be confirmed at the detailed design stage. Based on the above, the proposed reduced rear yard setback is considered appropriate.

**Minimum Front Façade Step-back, to not be required –**

Rationale: The minimum front façade step-back provision requires 45 degree angular plane at a height above 80% equivalent to the right-of-way, using 3 metre minimum step-backs. As noted above, the front lot line/front yard is considered to be Monsterrand Street. Therefore, a step-back would be required at a height of 16 metres for the apartment building. The apartment building is proposed at a height of 20 metres.

From a built form perspective and the orientation of the site plan, Veteran's Drive should be considered as the front yard when considering step-backs. The proposed 5 storey building would not require a stepback from Veterans Drive based on the width of the right of way. In looking at the building interface on Montserrand Street, directly across the street in a single storey plaza. The rear of this building faces the subject lands. The effect of increasing the height shall not negatively impact this existing use as that portion of the building is not utilized.

**Reduced minimum street level floor height – from 4.5m to 3.0m**

Rationale: The minimum street level floor height required in the MU2 Zone is 4.5m. This provision accounts for mixed use development providing an increased height for commercial use on the street level. The proposed development will have a residential use on the street level floor and only requires a minimum of 3.0m in height to accommodate.

**Increased building height – from 16.5m to 20m (townhouse – 11m, stacked towns – 14.5m, apartment – 20m)**

Rationale: The as of right permitted height in the MU Zone is 16.5m, which accommodates up to 4 storeys. The townhouses proposed will be 3 stories in height, approximately 11m. The stacked townhouses proposed will be 4 stories, approximately 14,5 metres in height, both conforming with the provision. However, the apartment, as proposed is 5 storeys in height, approximately 20 metres.

Increased height in the location proposed conforms with urban design guidelines and principles along arterial roads. The apartment building is sited at the corner of the site, at the intersection of Veterans Drive and Monsterrand Street. The building frames the street and provides increased density adjacent to the intensification corridor. The additional height also permits structured parking underneath the building which improves the site layout and additional greenspace opportunities that would have otherwise been required for surface parking. The project steps down in height with buffers closer to existing dwellings going east and south. The proposed increase in height, for an apartment building is appropriate in the given location.



### **Reduced landscape buffer areas – from 3.0m to 0.7m**

Rationale: The MU2 Zone requires that where any lot in the MU Zone abuts a Residential Zone, a continuous landscaped buffer area of 3m shall be provided along the abutting lot line of the lot, and a continuous tight board fence with a minimum height of 2m is to be constructed along the lot line. Landscape buffers are provided throughout the development, where required, with most accommodating the 3 metre requirement. However, along the south property line the landscape buffer is deficient at 0.7 metres at two locations where parking spaces are provided and the road stub ends. A landscape strip is proposed along this property line to provide a buffer. The 2m fence as required will be provide along the north, east and south property line.

### **To permit tandem parking**

Rationale: Tandem parking is requested for the townhouse and stacked townhouse dwellings to recognize that each dwelling has a minimum of two parking spaces which may be provided through a private garage parking space and a driveway parking space. As proposed, all townhouse dwellings will have a minimum of two parking spaces, with some having three private parking spaces, and approximately half of the ground related homes have side by side parking.

### **Reduced driveway length – from 6.0m to 5.5m**

Rationale: The zoning by-law requires a minimum driveway length of 6.0m (5.2.5.2 d), whereas 5.5m is provided throughout the development. The reduced length aligns with the required size of a parking spaces, thus the intent of the driveway to provide enough space to park a vehicle is maintained.

### **Permit accessory building and structures, specifically decks/balconies, in the front and side yard (along Monsterrand Street and Veterans Drive)**

Rationale: Keeping in line with the urban design guidelines and principles used throughout the design of the development, decks and balconies in the front and side yard are required. Decks and balconies are proposed on each façade of the apartment building, providing each dwelling unit with an outdoor amenity space. In addition, decks and balconies will become an architectural feature to the built form, providing fenestration and change of materials and glazing. Providing each dwelling unit with some private outdoor space is essential and seen as appropriate given the location of the site and built form.

### **Increased encroachment of accessory buildings and structures, specifically decks, in the exterior side yard and front yard – from 0.6m to 0m**

Rationale: The proposal, as shown, pushes the built form, both the stacked townhouses and the apartment building, close to the street, as suggested by Staff, in keeping with the urban design guidelines. Decks and balconies are proposed on each façade of the apartment building and the stacked townhouses, providing all dwelling units private outdoor space. As mentioned above, the apartment and stacked townhouses have a decreased side yard setback therefore the encroachment setbacks are required to be modified to align with the decreased setback requested. It is proposed that all decks and balconies can encroach up to the lot line, with 0m setback required. Given the location of the site and built form, and urban design guidelines, this is seen as appropriate.

### **Decrease in road width for a two-way travel condo road – from 6.4m to 6.0m**

Rationale: The development proposes a condo road of 6.0 metres, in keeping with the OBC Fire Code standard. The reduced width of the road continues to build on the urban design guidelines and principles and the walk ability of the site. Reduced road widths slow vehicular traffic, creating a more livable environment. The road will continue to operate, and functions as required while providing the necessary fire route. This is seen as appropriate given the development proposal.

### **To permit a Woonerf as a condo road, with a one-way paved road width of 3.0m**

Rationale: The development proposes to provide a one-way Woonerf, providing access to Blocks 1, 2 and 3, a total of 16 dwellings. This road, with a width of 3.0 metres, will act like a laneway, where speed of vehicles is drastically reduced to 10km/hr which allows people to use the area for other uses such as recreation. The road provides sufficient width to allow vehicles to easily access the double wide garages. The Woonerf will serve the intent of a one-way condo road and is seen as appropriate.

## **3.0 CONCLUSION**

This report explores the merits of the proposed development as it relates to all levels of applicable planning policy. The concept and associated application put forward would permit the development of 39 apartment units, 23 stacked townhouse dwellings and 19 standard townhouse dwellings, for a total of 78 units.

Parking, outdoor amenity space, landscaped areas, and pedestrian connections are all provided within the concept. The ZBA application submitted will permit the subject lands to be developed in a manner that will contribute to the City of Barrie's vision of a complete community.

The associated concept will be developed in accordance with the City's Urban Design guidelines. The proposal represents the compact and efficient built form encouraged through the OP and provides for a diversity of residential unit types and tenures supported through all levels of provincial and municipal planning policy. The concept is sensitive to the scale and character of the existing neighborhood and can be sustained through existing infrastructure and service levels without compromising the delivery of services to existing community residents.

The justification for approval of these applications is based on the conformity with the goals and objectives of the Provincial Policy Statement, Growth Plan for the Greater Golden Horseshoe, the City of Barrie Official Plan, and the City of Barrie Comprehensive Zoning By-law 2009-141. It is our professional opinion that the following Zoning Bylaw Amendment application represents good planning.

Respectfully submitted,  
**Innovative Planning Solutions**



**Darren Vella, MCIP, RPP**  
President & Director of Planning



**Vanessa Simpson, BID, MPI**  
Senior Planner

## **APPENDICES**

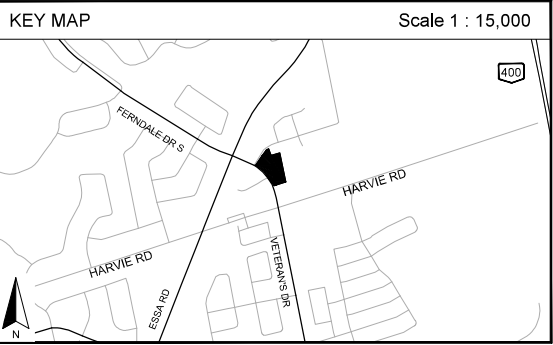
## **APPENDIX A: CONCEPTUAL SITE PLAN**



ZONING TABLE - MU2-XX			
PROVISION		REQUIRED	PROVIDED
Lot Area (min.)		-	8,944.60m²
Lot Frontage (min.)		-	70.18m (Montserrat St)
Front Yard Setback	Residential use abutting lot frontage (min.)	3.0m	4.1m (to Montserrat St)
	Min.	-	1.8m
Side Yard Setback	Min. abutting street or laneway	3.0m	0.5m (to Veterans Dr)
	Max.	-	-
Rear Yard Setback (min.)	Abutting Street or Laneway	1.5m	n/a
	Abutting Residential, OS or EP Zone	7.0m	5.0m
Front Facade Step-back (min.)		45 degree angular plane at height above 80% equivalent right-of-way using 3m minimum step-backs; therefore, since Montserrat is considered to be the front lot line/yard, a step-back is required at 16m height	Not required
Side Facade Step-back (min.)		If adjacent to an OS zone, 5.5m at height above 80% equivalent of right-of-way	n/a
Rear Facade Step-back (min.)		45 degree angular plan above 7.5m using minimum 3m step-backs	n/a
Lot Coverage (max.)		-	50%
Gross Floor Area (max.)		-	135%
Street Level Floor Height (min.)		4.5m	3.0m
Building Height	Min.	7.5m	7.5m
	Max.	16.5m	Towns - 11m Stacked towns - 14.5m Apartment - 20m
Dwelling Unit Floor Area (min.)		45m² / 1 bedroom + 10m² per bedroom	45m² / 1 bedroom + 10m² per bedroom
Outdoor Amenity Area (min.) unconsolidated		12m²/unit	>12m²/unit unconsolidated
Landscape Buffer Areas (min.) (5.4.4.0)		3.0m	0.7m
Parking Spaces (min.) - ratio		1 space / unit (78 spaces)	1 space / unit (147 spaces)
Barrier Free Parking Spaces (min.)		4 BF; 2 Type A and 2 Type B	4 BF; 2 Type A and 2 Type B
Tandem Permitted		No	Yes
Parking for Apartment Dwellings (5.4.4.1)		35% max lot coverage	<35%
Driveway Length (5.2.5.2 d) (min.)		6.0m	5.5m

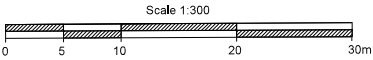
ZONING TABLE - Accessory Buildings & Structures		
PROVISION	REQUIRED	PROVIDED
Height (max.)	4m	<4m
Occupy any part of a front yard	Not permitted	Permitted. Decks / balconies occupy the front yard (Montserrat St)
Occupy any part of a side yard	Not permitted	Permitted. Decks / balconies occupy the side yard (Veterans Dr)
Rear lot line setback (min.)	0.6m	>0.6m
Side lot line setback (min.)	0.6m	0.0m to Veterans
Front lot line setback (min.)	-	0.0m to Montserrat

Note: The proposed Zoning By-law Amendment Special Provisions provide for greater flexibility within the design and development proposal. Through Site Plan process the development will be further refined.



## CONCEPTUAL SITE PLAN VETERAN'S LANE

PART LOTS 6, CONCESSION 13  
339 VETERANS DRIVE & 341 VETERANS LANE  
IN THE  
CITY OF BARRIE



### LEGEND

- SUBJECT LANDS
- LANDSCAPED OPEN SPACE

### DWELLING UNIT BREAKDOWN

- TYPE A & B TOWNS = 19 UNITS
- TYPE C STACKED TOWNS = 20 UNITS
- TYPE D APARTMENT = 39 UNITS

Source: Site Plan is designed by Hunt Design Associates and used with permissions to provide a zoning analysis and matrix.

# CONCEPTUAL SITE PLAN - 78 UNITS "VETERAN'S LANE" - CITY OF BARRIE

RESIDENTIAL	CURRENT OP DESIGNATION
RH - 339 R1 - 341	CURRENT ZONE

SCHEDULE OF REVISIONS			
No.	Date	Description	By
1.	1 May 2020	Revised concept plan for resubmission	VS

<b>IPS</b> INNOVATIVE PLANNING SOLUTIONS PLANNERS • PROJECT MANAGERS • LAND DEVELOPERS 150 DUNLOP STREET EAST, SUITE 201, BARRIE, ONTARIO L4M 1B1 Tel: 705 • 812 • 3281 fax: 705 • 812 • 3438 e: info@ipsconsultinginc.com www.ipsconsultinginc.com	
Date:	October 1, 2019
File:	Veteran's Lane
Drawn By:	VS
Reviewed By:	DV

## **APPENDIX B: CITY OF TORONTO, ROAD ENGINEERING DESIGN GUIDELINES**

## Lane Widths Guideline Quick Reference

This document is intended to be used as a quick reference for individuals familiar with the Lane Width Guidelines and shall be used in conjunction with the Lane Width Guidelines.

			Minimum (m)	Target (m)	Maximum (m)	TTC		High Truck Volume	Horizontal Alignment Curves
						TTC Bus Routes	TTC Streetcar Routes		
Through Lane	60km/h or more		3.0	3.0	3.5	x	+ <sup>1</sup>	+	+
	50km/h			3.0	3.3				
	40km/h or less			3.0	3.0				
Curb Lane	Shared Curb Lane without Urban Shoulder		3.3	4.3	4.3	+ <sup>2</sup>	x	+	+
	Shared Curb Lane with Urban Shoulder or Curb Lane with Dedicated Cycling Facility	60km/h or more	3.0	3.5	3.5				
		50km/h		3.3	3.5				
		40km/h or less		3.3	3.5				
Urban Shoulder			1.2	2.3	2.3				
Two-way Left Turn Lane			3.0	3.0	3.3	x	x	+	+
Dedicated Left Turn Lane			3.0	3.0	3.3	x	x	+	+
Dedicated Right Turn Lane			3.0	3.0	3.3	+	x	+	+
Dedicated Parking Lane			2.0	2.4	2.8	x	x	x	+
Dedicated Cycling Facility			Note 1						

### Urban Shoulders

An urban shoulder is a space, delineated by an edge line that a cyclist may choose to ride in instead of riding in the vehicular shared curb lane where dedicated cycling facilities are not provided. An urban shoulder is not an alternative to a dedicated cycling facility. An urban shoulder should be provided wherever possible. An urban shoulder delineated by an edge line shall be a minimum width of 1.2m.

Note 1 – Refer to Ontario Traffic Manual Book 18: Cycling Facilities

<sup>1</sup> Through lanes should be a minimum width of 3.1m on TTC streetcar routes.

<sup>2</sup> Curb lanes should be a minimum width of 3.3m on TTC bus service routes.



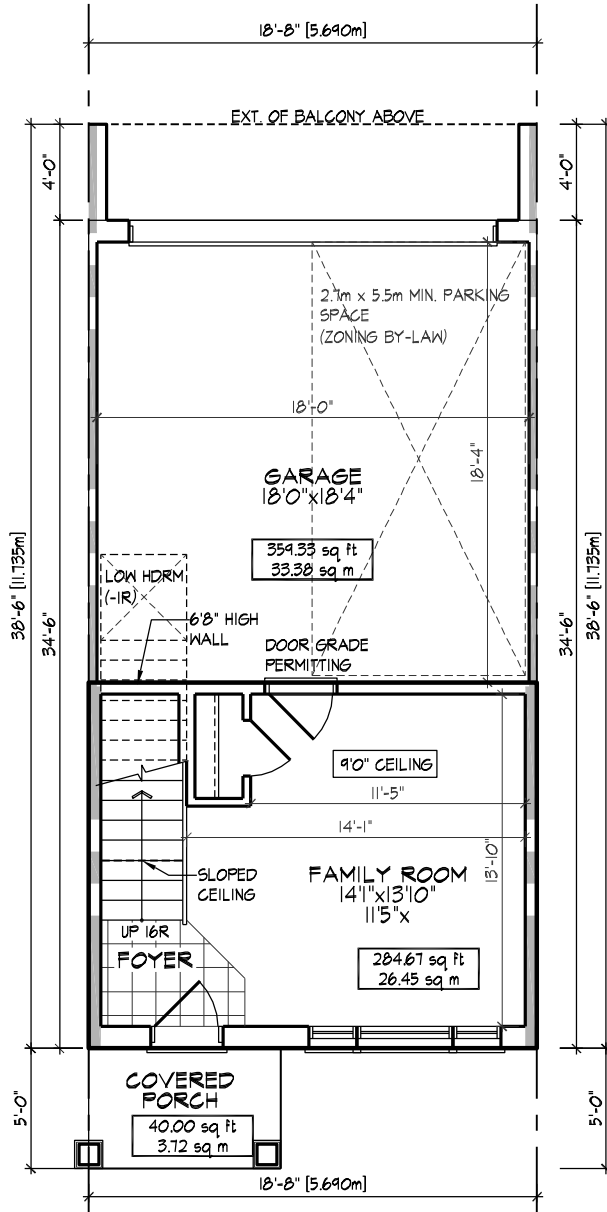
Lane width allocation and prioritization

Lane types and widths have been prioritized into a list of items to determine which lanes and widths should be provided at locations where there is insufficient road width to accommodate the desired cross section. The following list of items have been prioritized from most important to least important. Items should be allocated to cross section in order of priority. Any specific item should not be allocated to a cross section until all items with a higher priority have been provided.

Lane Width Priorities from Most Important to Least Important

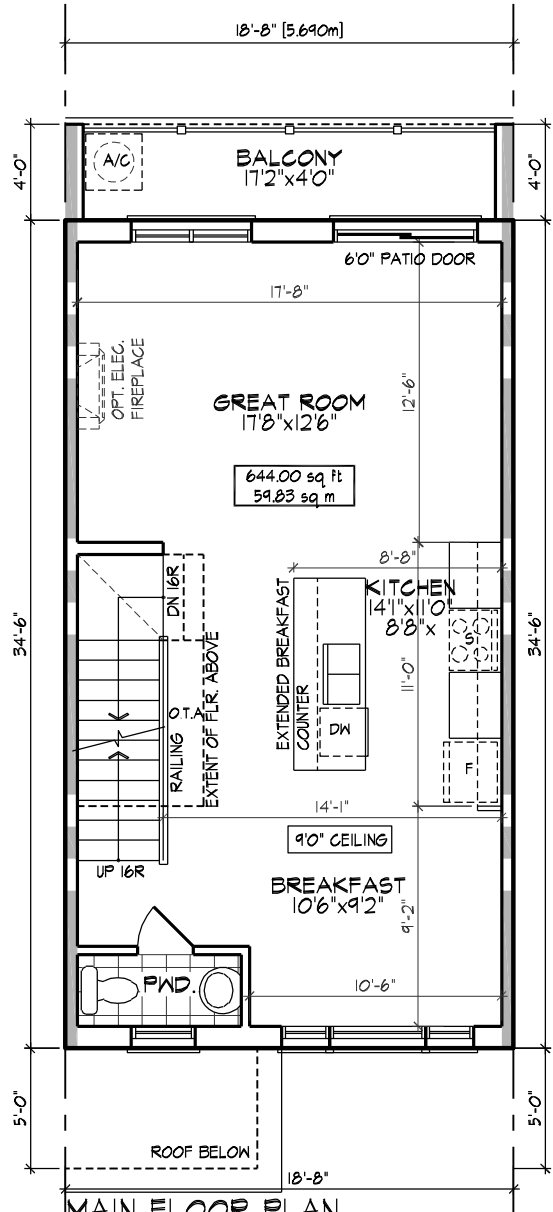
1. Minimum Curb Lane Width
2. Minimum Through Lane Width
3. Minimum Turn Lane Width
4. 3.3m Wide Curb Lane Width
5. Minimum Urban Shoulder Width if possible
6. Minimum Conventional Bicycle Lane Width where identified
7. 3.5m Wide Curb Lane Width on 60km/h roads
8. Minimum Separated Bicycle Lane Width
9. Target Curb Lane with Urban Shoulder or Cycling Facility Width
10. Target Through Lane Width
11. Desired Bicycle Lane Width
12. Target Curb Lane Width
13. Target Turn Lane Width
14. Curb Lane Width Above Target if required
15. Through Lane Width Above Target if required
16. Turning Lane Width Above Target if required

## **APPENDIX C: ARCHITECTURAL FLOOR PLANS**



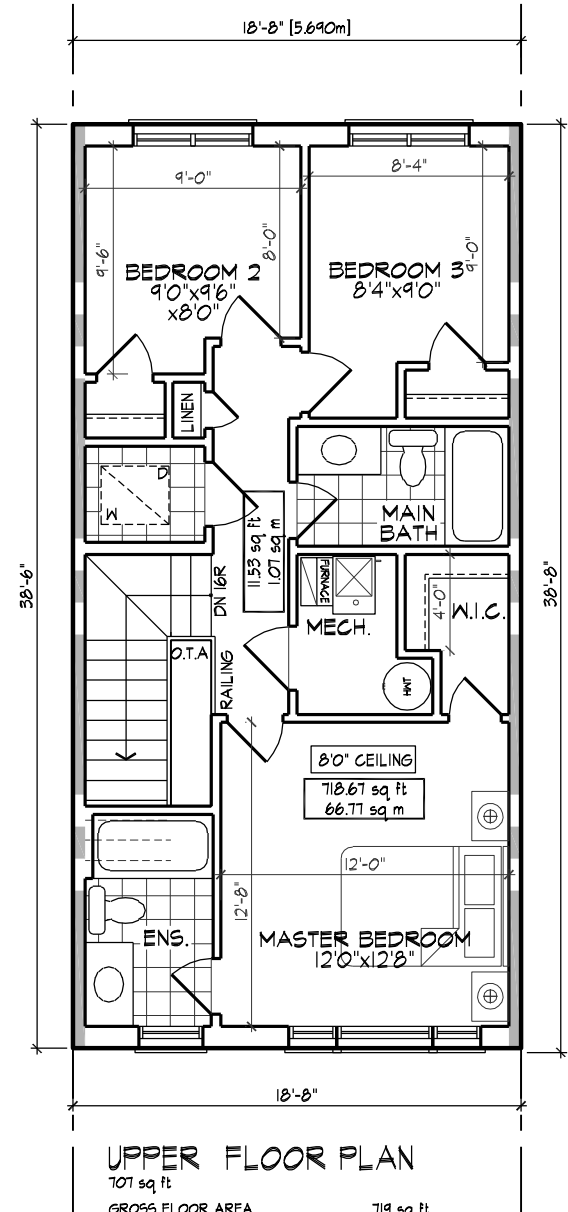
LOWER FLOOR PLAN

285 sq ft  
COVERAGE W/O PORCH 644 sq ft  
COVERAGE W/ PORCH 684 sq ft



MAIN FLOOR PLAN

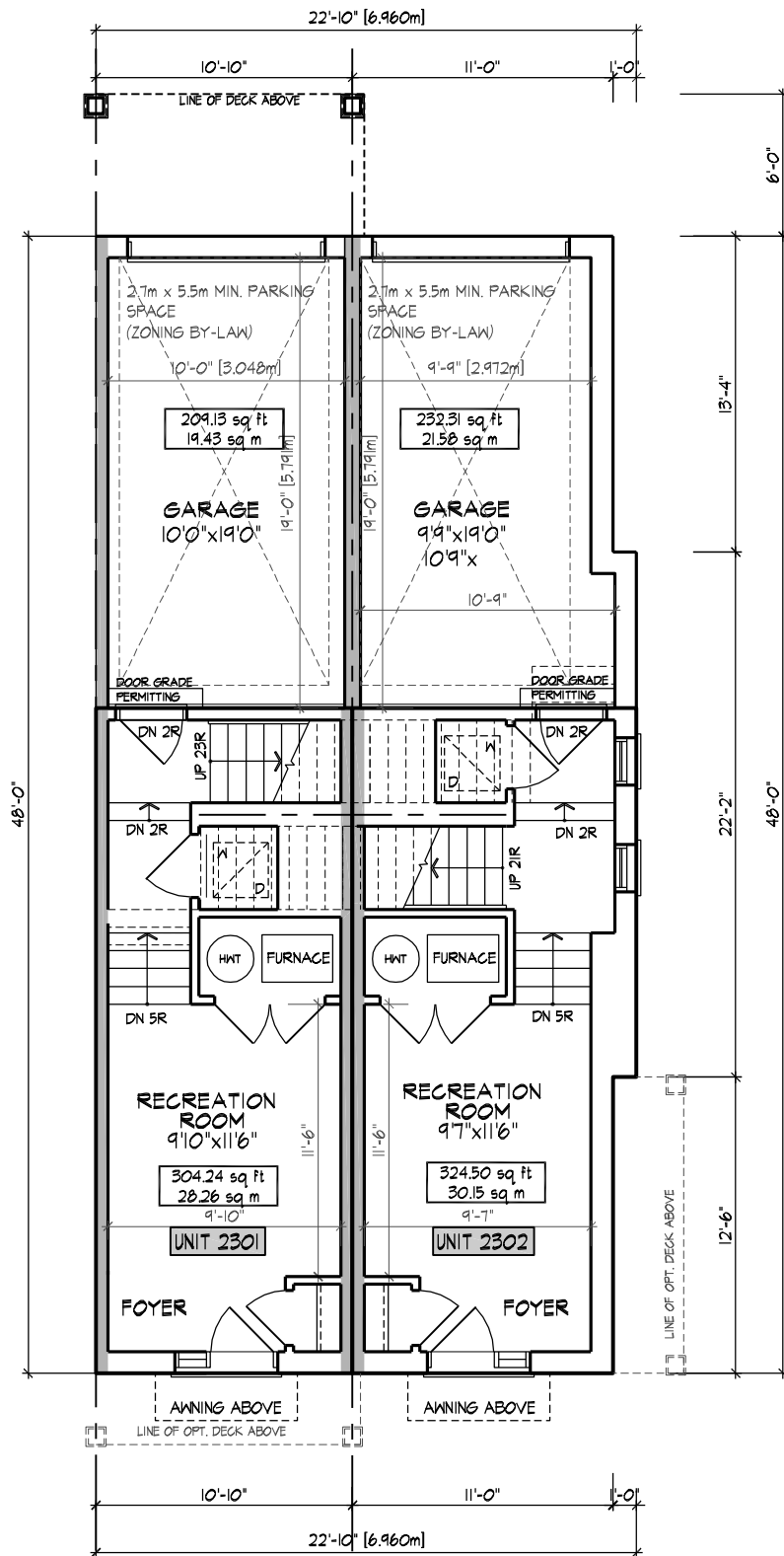
644 sq ft  
GROSS FLOOR AREA 644 sq ft  
DEDUCT OPEN AREAS 0 sq ft  
NET AREA 644 sq ft



UPPER FLOOR PLAN

707 sq ft  
GROSS FLOOR AREA 719 sq ft  
DEDUCT OPEN AREAS 12 sq ft  
NET AREA 707 sq ft



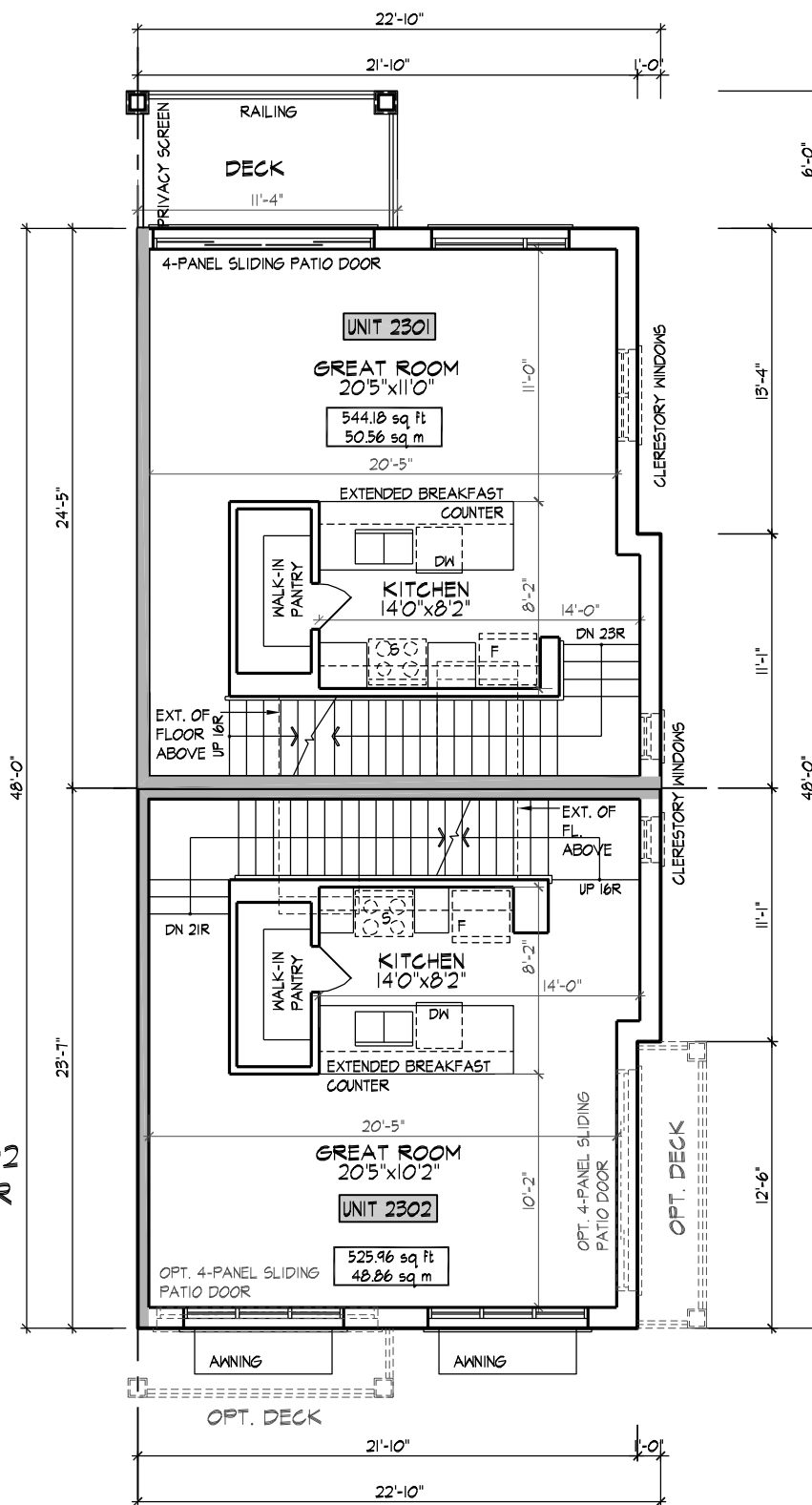


UNIT 2301  
GROUND FLOOR PLAN

304 sq ft			
COVERAGE W/O PORCH	513 sq ft	COVERAGE W/O PORCH	551 sq ft
COVERAGE W/ PORCH	513 sq ft	COVERAGE W/ PORCH	551 sq ft

UNIT 2302  
GROUND FLOOR PLAN

UNIT 2301 & UNIT 2302  
PARTIAL MAIN FLOOR  
PLAN W/OPT. PWD

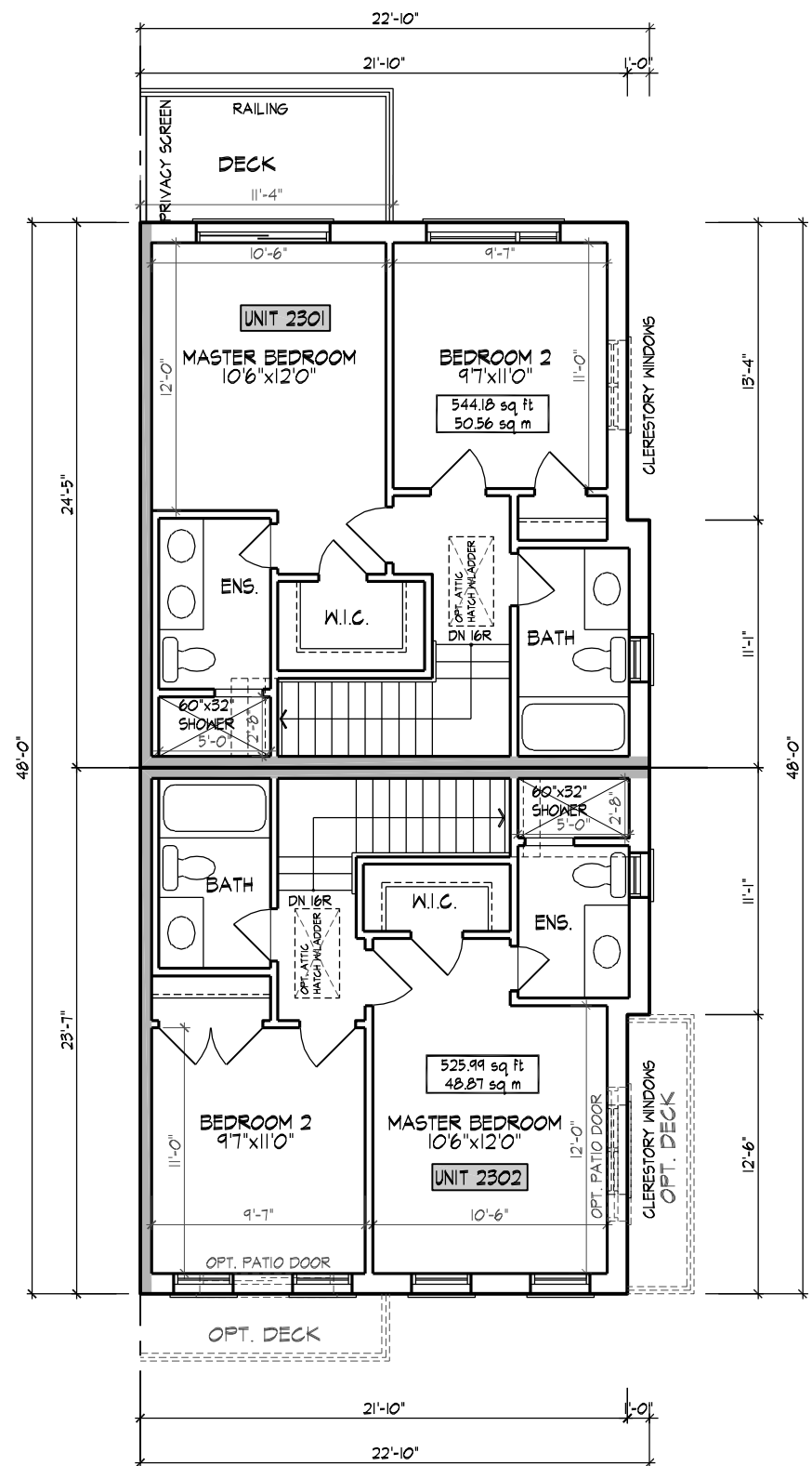


UNIT 2301  
MAIN FLOOR PLAN,

544 sq ft			
GROSS FLOOR AREA	544 sq ft		
DEDUCT OPEN AREAS	0 sq ft		
NET AREA	544 sq ft		

UNIT 2302  
MAIN FLOOR PLAN,

526 sq ft			
GROSS FLOOR AREA	544 sq ft	526 sq ft	
DEDUCT OPEN AREAS	0 sq ft	0 sq ft	
NET AREA	544 sq ft	526 sq ft	



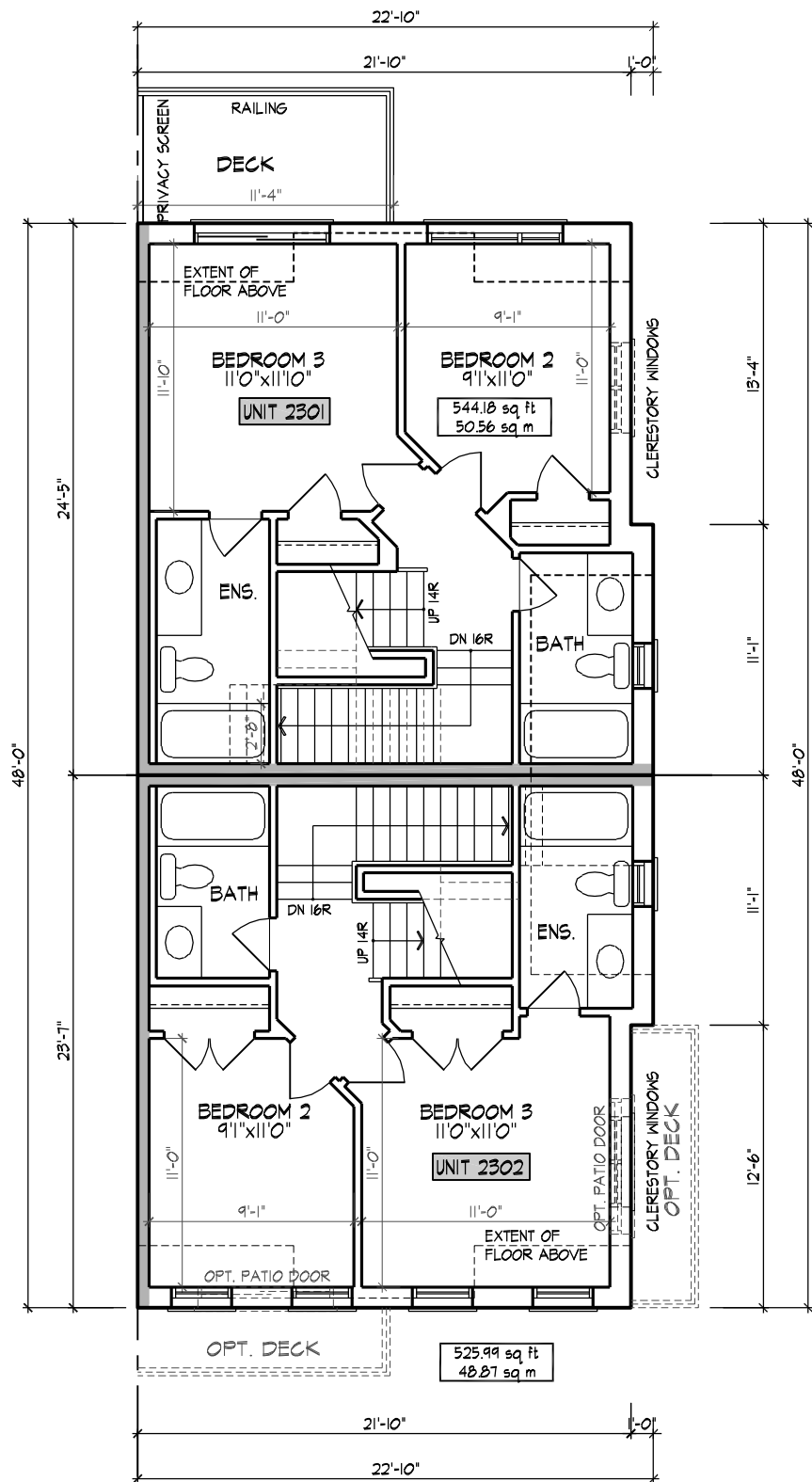
UNIT 2301  
SECOND FLOOR PLAN,

544 sq ft			
GROSS FLOOR AREA	544 sq ft		
DEDUCT OPEN AREAS	0 sq ft		
NET AREA	544 sq ft		

UNIT 2302  
SECOND FLOOR PLAN,

526 sq ft			
GROSS FLOOR AREA	544 sq ft	526 sq ft	
DEDUCT OPEN AREAS	0 sq ft	0 sq ft	
NET AREA	544 sq ft	526 sq ft	

BACK TO BACK TOWNHOUSE (TYPE A)

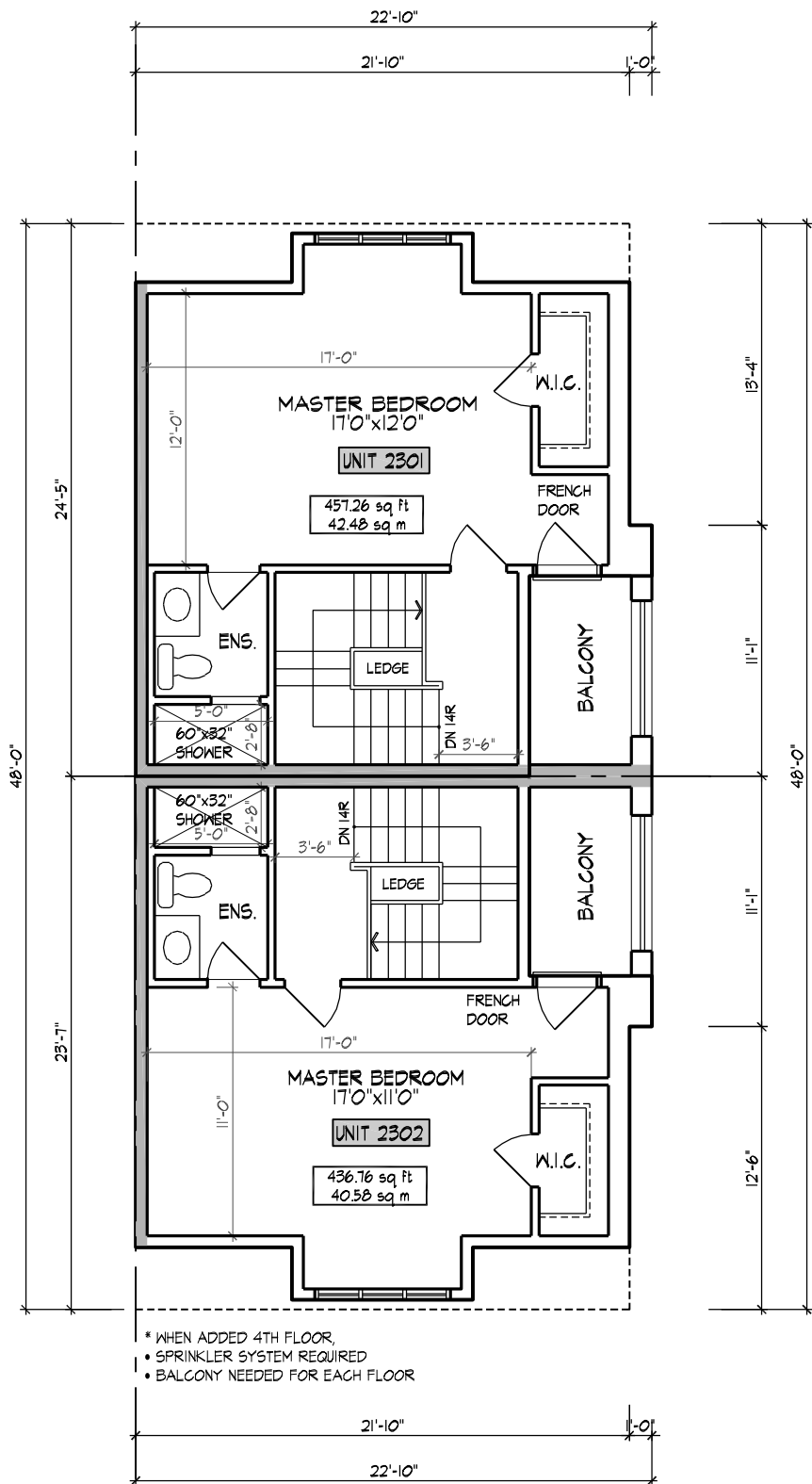


UNIT 2301  
OPT. SECOND FLOOR PLAN,  
544 sq ft

GROSS FLOOR AREA	544 sq ft
DEDUCT OPEN AREAS	0 sq ft
NET AREA	544 sq ft

UNIT 2302  
OPT. SECOND FLOOR PLAN,  
526 sq ft

GROSS FLOOR AREA	526 sq ft
DEDUCT OPEN AREAS	0 sq ft
NET AREA	526 sq ft



UNIT 2301  
OPT. THIRD FLOOR PLAN,  
451 sq ft

GROSS FLOOR AREA	451 sq ft
DEDUCT OPEN AREAS	0 sq ft
NET AREA	451 sq ft

UNIT 2302  
OPT. THIRD FLOOR PLAN,  
437 sq ft

GROSS FLOOR AREA	437 sq ft
DEDUCT OPEN AREAS	0 sq ft
NET AREA	437 sq ft

- \* WHEN ADDED 4TH FLOOR,
- SPRINKLER SYSTEM REQUIRED
- BALCONY NEEDED FOR EACH FLOOR

**APPENDIX D: MEMORANDUM FROM KRCMAR SURVEYORS LTD. AND THE SITE PLAN WITH  
THE DRAFT PLAN OF SUBDIVISION OVERLAY**



**Sean Mason Homes**

7100 – 3100 Steeles Ave. East  
Markham, Ont. L3R 8T3

**May 26<sup>th</sup>, 2020**

Attention: **Mr. Sean Mason,**

Re: **1-Block Plan of Subdivision – Veteran's Lane**  
City of Barrie

Hello Mr. Mason,

I am writing to inquire on the purpose of the 1-Block Plan of Subdivision that was prepared by Innovative Planning Solutions as part of the Veteran's Lane condominium development. I would respectfully suggest that the development process may be simplified by the elimination of the 1-Block subdivision entirely, and still achieve the identical development objectives.

As I understand it, this 1-Block Plan of Subdivision is intended to provide a vehicle to convert the site's lot/concession lands onto a registered plan, to allow for part-lot control exemption by-law passing. This strategy may have been entirely appropriate based on previous site plan designs, however, the proposed location of the common element 3-storey townhouses is mostly within the east parcel which lands are already described on Registered Plan 1200 in the City of Barrie.

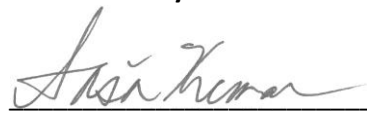
The Veteran's Lane redevelopment, per May 1, 2020 site plan, consists of three(3) types of condominium products: (1) mid-rise units within the triangular parcel; (2) stacked towns within the triangular parcel/Veteran's Lane; and (3) common element 3-storey townhouses mostly within Lot 1, Registered Plan 1200 (former single family lot).

I would recommend, with the support of the City of Barrie and your Planning Consultant, the following:

- Elimination of the 1-Block Plan of Subdivision both to simplify the development process, and because it's not needed (majority of proposed common element townhouse site is already on a Registered Plan);
- Develop 2 or 3 condominium corporations (mid-rise; stacked; common element) utilizing site plan/draft plan of condominium process for municipal approval;
- Advise City of Barrie Planning Department that a small 1.4 metre portion of Townhouse Block 1 may require C of A Severance Application, to meet planning Act requirements for POTLs creation within a common element condominium (1.4 metres of Block 1 crosses into current Veteran's Lane lands);
- Veterans Drive road widening dedication (2.76 metres) be included in Site Plan Agreement requirements.

Regards,

**Krcmar Surveyors Ltd.**



(Mr.) Saša Krcmar, OLS, MBA

1137 Centre Street Thornhill Ontario, L4J 3M6  
905.738.0053 F 905.738.9221 www.krcmar.ca

**s u r v e y o r s**

1137 Centre Street Thornhill Ontario, L4J 3M6  
905.738.0053 F 905.738.9221 www.krcmar.ca

**s u r v e y o r s**

**K R C M A R**



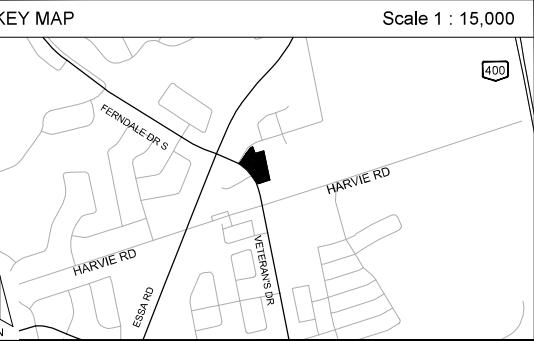
Note: The proposed Zoning By-law Amendment Special Provisions provide for greater flexibility within the design and development proposal. Through Site Plan process the development will be further refined.

# CONCEPTUAL SITE PLAN with Draft Plan overlay

## "VETERAN'S LANE" - CITY OF BARRIE

RESIDENTIAL	CURRENT OP DESIGNATION
RH - 339 R1 - 341	CURRENT ZONE

SCHEDULE OF REVISIONS			
No.	Date	Description	By
1.	1 May 2020	Revised concept plan for resubmission	VS
2.	26 May 2020	Draft Plan of Subdivision overlaid for review and discussion	VS

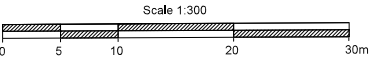


# CONCEPTUAL SITE PLAN

## with Draft Plan overlay

### VETERAN'S LANE

PART LOTS 6, CONCESSION 13  
339 VETERANS DRIVE & 341 VETERANS LANE  
IN THE  
CITY OF BARRIE



- LEGEND
- SUBJECT LANDS
  - LANDSCAPED OPEN SPACE
- DWELLING UNIT BREAKDOWN
- TYPE A & B TOWNS = 19 UNITS
  - TYPE C STACKED TOWNS = 20 UNITS
  - TYPE D APARTMENT = 39 UNITS

Source: Site Plan is designed by Hunt Design Associates and used with permissions to provide a zoning analysis and matrix.



**INNOVATIVE PLANNING SOLUTIONS**  
PLANNERS • PROJECT MANAGERS • LAND DEVELOPERS  
150 DUNLOP STREET EAST, SUITE 201, BARRIE, ONTARIO L4M 1B1  
Tel: 705 • 812 • 3281 fax: 705 • 812 • 3438 e: info@ipsconsultinginc.com www.ipsconsultinginc.com

Date:	October 1, 2019	Drawn By:	VS
File:	Veteran's Lane	Reviewed By:	DV

## **APPENDIX E: DRAFT ZONING BY-LAW AND SCHEDULE 'A'**

## **THE CITY OF BARRIE COMPREHENSIVE ZONING BY-LAW NUMBER 2009-141**

"A By-law of the City of Barrie to amend Comprehensive Zoning By-Law No. 2009-141 by rezoning lands described as Lot 1, Registered Plan 1200, and Part 2, Registered Plan 51R-22069 in the City of Barrie, County of Simcoe, known municipally as 339 Veterans Drive, 341 Veterans Lane, and parcel Roll No. 434204001801060 from the Residential Single Detached Dwelling First Density (R1), Residential Single Detached Dwelling Second Density (R2) and Residential Hold (RH) Zones to the Mixed Use Corridor Zone with Exceptions (MU2-SPXX)."

**WHEREAS** the Council of the Corporation of the City of Barrie may pass by-laws pursuant to Section 34 of the Planning Act, R.S.O 1990, as amended;

**AND WHEREAS** the Council of the Corporation of the City of Barrie has determined a need to rezone a parcel of land known municipally as 339 Veterans Drive, 341 Veterans Lane, and parcel Roll No. 434204001801060;

**AND WHEREAS** the Council of the Corporation of the City of Barrie deems the said application to be in conformity with the Official Plan of the City of Barrie, as amended, and deems it advisable to amend By-law 2009-141.

### **NOW THEREFORE THE COUNCIL OF THE CORPORATION OF THE CITY OF BARRIE HEREBY ENACTS AS FOLLOWS:**

1. **THAT** the Zoning By-Law Map, is hereby further amended by rezoning those lands described as Lot 1, Registered Plan 1200, and Part 2, Registered Plan 51R-22069 in the City of Barrie, County of Simcoe, known municipally as 339 Veterans Drive, 341 Veterans Lane and parcel Roll No. 434204001801060, the Residential Single Detached Dwelling First Density (R1), Residential Single Detached Dwelling Second Density (R2) and Residential Hold (RH) Zones to the Mixed Use Corridor Zone with Exceptions (MU2-SPXX) as shown in Schedule "A" attached hereto, and Schedule "A" attached hereto forms part of By-Law 2000-02 as amended;
2. **THAT** the following additions shall be to the Mixed Use Corridor Zone with Exceptions (MU2-SPXX) Zone:
  - i. A minimum side yard setback, abutting a street or laneway, of 0.5m shall be permitted;
  - ii. A minimum rear yard setback, abutting a Residential, OS or EP Zone of 5.0m shall be permitted;
  - iii. A minimum front façade step-back shall not be permitted;
  - iv. A minimum street level floor height of 3.0m shall be permitted; and
  - v. A maximum height of 20 metres shall be permitted;
  - vi. A landscaped buffer area, where any lot in a Mixed Use Zone abuts a lot in a Residential Zone, a continuous landscaped buffer area of a minimum of 0.7m shall be permitted;

- vii. Tandem parking shall be permitted;
- viii. A minimum driveway space length of 5.5m shall be permitted;
- ix. Accessory building and structures, shall be permitted in the front and side yards;
- x. Increased encroachment of accessory buildings and structures, specifically decks, in the exterior side yard and front yard, 0.0m shall be permitted;
- xi. A minimum width of asphalt road width for a condo road of 6.0 metres shall be permitted; and
- xii. A Woonerf as a condo road, with a one-way pave road width of 3.0 metres shall be permitted.

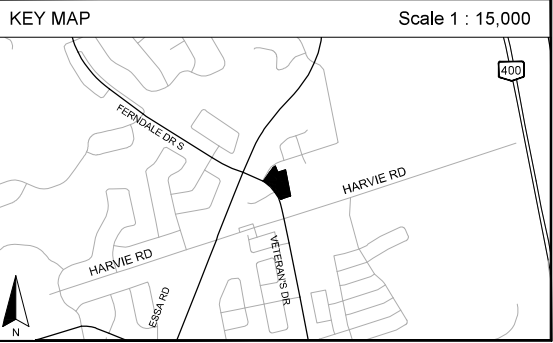
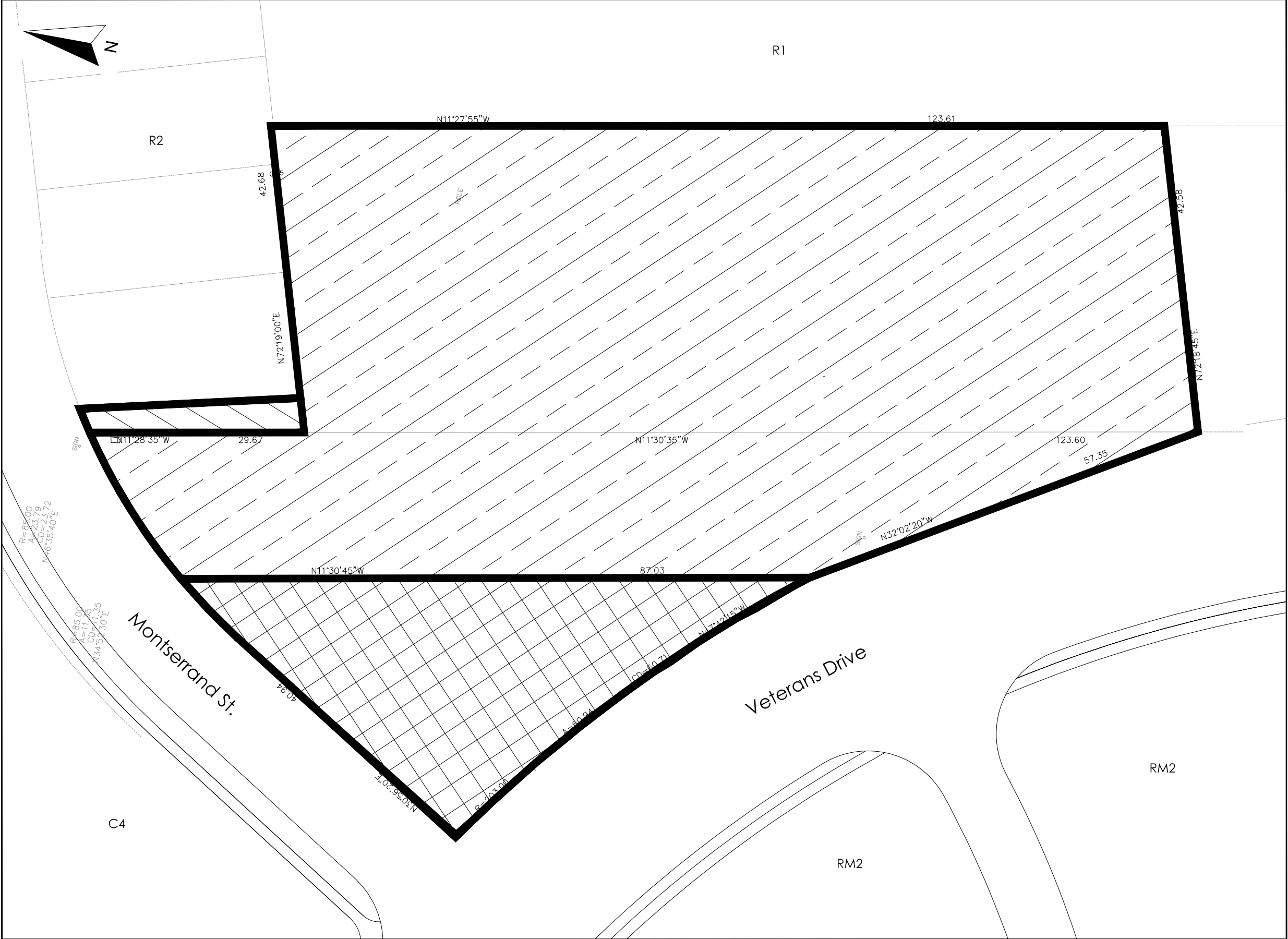
3. **THAT** this by-law shall take effect as of the date of passing, subject to the provisions of the Planning Act, R.S.O. 1990, Chap. P.13 as amended.

**BY-LAW** read a **FIRST**, **SECOND**, and **THIRD** time and finally **PASSED** this \_\_\_\_ day of \_\_\_\_\_ 2020.

**The Corporation of the City of Barrie**

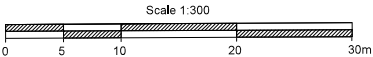
\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Clerk



**SCHEDULE 'A'**  
**ZONING BY-LAW AMENDMENT**

PART LOTS 6, CONCESSION 13  
339 & 341 VETERAN'S DRIVE  
IN THE  
CITY OF BARRIE



**LEGEND**

- SUBJECT LANDS
- LANDS TO BE REZONED FROM RESIDENTIAL HOLDING ZONE (RH) TO MIXED USE CORRIDOR SPECIAL PROVISION (MU2 (SP-XX))
- LANDS TO BE REZONED FROM RESIDENTIAL SINGLE DETACHED DWELLING FIRST DENSITY (R1) TO MIXED USE CORRIDOR SPECIAL PROVISION (MU2 (SP-XX))
- LANDS TO BE REZONED FROM RESIDENTIAL SINGLE DETACHED DWELLING SECOND DENSITY (R2) TO MIXED USE CORRIDOR SPECIAL PROVISION (MU2 (SP-XX))

Clerk

Mayor

**SCHEDULE 'A' - ZONING BY-LAW AMENDMENT \_\_\_\_.**  
**"VETERAN'S LANE" - CITY OF BARRIE**

SCHEDULE OF REVISIONS			
No.	Date	Description	By

**INNOVATIVE PLANNING SOLUTIONS**  
PLANNERS • PROJECT MANAGERS • LAND DEVELOPERS  
647 Welham Road, Unit 9A, Barrie, Ontario, L4N 0B7  
Tel: 705 • 812 • 3281 fax: 705 • 812 • 3438 e: info@ipsconsultinginc.com www.ipsconsultinginc.com

Date:	May 21, 2020	Drawn By:	VS / AS
File:	Veteran's Lane	Reviewed By:	DV