



PLANNING RATIONALE

80 BIG BAY POINT ROAD
CITY OF BARRIE | COUNTY OF SIMCOE
PREPARED FOR: TONLU HOLDINGS LIMITED

AUGUST 2021



INNOVATIVE PLANNING SOLUTIONS
PLANNERS • PROJECT MANAGERS • LAND DEVELOPMENT

TABLE OF CONTENTS

1.0 INTRODUCTION	4
2.0 LOCATION & CONTEXT	7
2.1 Subject Site	8
2.2 Surrounding Context	8
3.0 DESCRIPTION OF THE PROPOSAL	14
3.1 Description of the Draft Plan of Subdivision	15
3.2 City of Barrie Official Plan	17
3.3 City of Barrie Zoning By-law 2009-141	17
3.4 Supporting Studies	20
4.0 PLANNING ANALYSIS	23
4.1 Provincial Policy Statement (2020)	24
4.2 Places to Grow - Growth Plan for the Greater Golden Horseshoe (2020)	27
4.3 Lake Simcoe Protection Plan	29
4.4 City of Barrie Official Plan	32
5.0 CONCLUSION	37
5.1 Conclusion	39

LIST OF FIGURES

Figure 1. Subject Site	5
Figure 2. Aerial View	8
Figure 3. Official Plan - Schedule A Land Use	10
Figure 4. Official Plan - Schedule E Road Widening Plan	10
Figure 5. Official Plan - Schedule F Conservation Authority Regulation Limits and Watercourses	11
Figure 6. City of Barrie Zoning By-law 2009-141	11
Figure 7. Draft Plan of Subdivision	16
Figure 8. General Industrial (GI) Zone - Barrie Zoning By-law 2009-141	17
Figure 9. Industrial Standards Chart Excerpt from Zoning By-law 2009-141	19
Figure 10. Schedule 2 - A Place to Grow Concept	28
Figure 11. Lake Simcoe Protection Act Watershed Boundary	30
Figure 12. Official Plan - Schedule H Natural Heritage Resources	33
Figure 13. Official Plan Schedule B - Planning Areas	34

INTRODUCTION



Innovative Planning Solutions has been retained by Tonlu Holdings Limited to complete a Planning Justification Report in support of an application for Draft Plan of Subdivision to permit the development of an industrial subdivision containing 31 lots and 2 blocks (1 Stormwater Management Pond Block and 1 Environmental Protection Block) in Two Phases as well as the creation of a new municipal road identified as “Street A”, on lands known municipally as 80 Big Bay Point Road, City of Barrie. See Figure 1.

The subject lands possess frontage on different municipal roads (Big Bay Point Road and Bayview Drive). The properties herein will be referred to as the ‘subject lands’. The lands possess a total area of 15.6 ha (38.5 acres) with 230.75 metres (757 feet) of frontage along Bayview Drive and 232 metres (761 feet) of frontage along Big Bay Point Road.

This report is to provide an assessment of the applicable planning policy framework and provide opinions supporting the conclusion that approving the requested application conforms to and is consistent with applicable policy and plan documents and represents good planning in the public interest.

This Report will review the merits of the development proposal in the context of applicable Provincial and Municipal policies to provide necessary rationale for the approval of the proposed development. This Report will review the following documents to demonstrate consistency:

- Provincial Policy Statement (2020)
- Places to Grow - Growth Plan for the Greater Golden Horseshoe (2020)
- Lake Simcoe Protection Plan (2009)
- City of Barrie Official Plan (Office consolidation – January 2018)
- City of Barrie Zoning By-law (Office consolidation – January 2020)



Figure 1. Subject Site

Based on the results of the technical analysis and on an evaluation of the applicable planning policy noted above, the proposed applications:

- are consistent with the applicable policies of the Provincial Policy Statement 2020;
- conform with the applicable policies of the Growth Plan for the Greater Golden Horseshoe 2020;
- conform with the applicable policies of the Lake Simcoe Protection Plan;
- conform with the applicable policies of the City of Barrie Official Plan (Office consolidation - January 2018);
- comply with City of Barrie Zoning By-law (Office consolidation - January 2020);
- do not negatively impact significant / key natural heritage and key hydrologic features, functions and areas, or the Provincial Natural Heritage System;
- do not contain, adjoin or impact significant cultural heritage / archaeological resources;
- do not contain, adjoin or are impacted by any natural or human-made hazards; and,
- represents good planning.

Details regarding the proposed development and application are provided in the following sections of this Report.

LOCATION & CONTEXT



2

2.1 Subject Site

The subject lands are located at 80 Big Bay Point Road and 315 Bayview Drive possessing frontage on both municipal roads (Big Bay Point Road and Bayview Drive). The lands are L-shaped and are in close proximity to the major intersection of Big Bay Point Road and Bayview Drive. The lands possess a total area of 15.6 ha (38.5 acres) with 230.75 metres (757 feet) of frontage along Bayview Drive and 232 metres (761 feet) of frontage along Big Bay Point Road.

Along the frontage of Big Bay Point Road exists the Bayview Spur rail line that connects into the Barrie Collingwood Railway (BCRY). One gated entrance is in the southwest corner of the property fronting Big Bay Point that crosses the spur line and one existing entrance is located on the southern most portion of the Bayview Drive frontage.

As identified on Schedule D Road Plan and Schedule E Road Widening Plan of the Official Plan, Big Bay Point Road is identified as an Arterial Road with a planned right-of-way width of 41m and Bayview Drive is identified as a Major Collector with a planned right-of-way width of 27m.

The subject lands are designated 'General Industrial' as per Schedule A Land Use in the Official Plan. Figure 3 illustrates the Official Plan designation of the subject lands and lands in the immediate vicinity. The lands are located

within the 400 East Industrial Planning Area, as per Schedule B Planning Areas in the Official Plan.

The lands are within the Lake Simcoe Regional Conservation Authority jurisdiction as per Schedule F of the Barrie Official Plan and contain regulated areas which include a watercourse that runs along the north and western property lines of the subject lands. The lands are relatively flat except for the areas to the west and north that contain an existing watercourse. The lands do possess sporadic vegetation and show signs of previous clearing based on the size of vegetation present on site.

The subject lands are currently zoned 'General Industrial (GI) Zone' as per the City of Barrie Zoning By-law 2009-141. Figure 6 illustrates the current zoning of the subject lands and that of properties within the general vicinity.

2.2 Surrounding Context

As per Schedule A Land Use of the City of Barrie Official Plan, the subject site resides within a large area designated General Industrial. Schedule B Planning Areas also illustrates the site within a large Industrial planning area. No sensitive land uses (residential or otherwise) are located within proximity to the subject site. The closest residential neighbourhoods are in excess of 1 kilometre to the west of the site, and another community approximately 1 kilometre north of the site.



Figure 2. Aerial View



Image 1 - Existing access from Bayview Drive



Image 2 - Existing access from Big Bay Point Road over BRCY



Image 3 - Aerial Birds Eye View Looking North

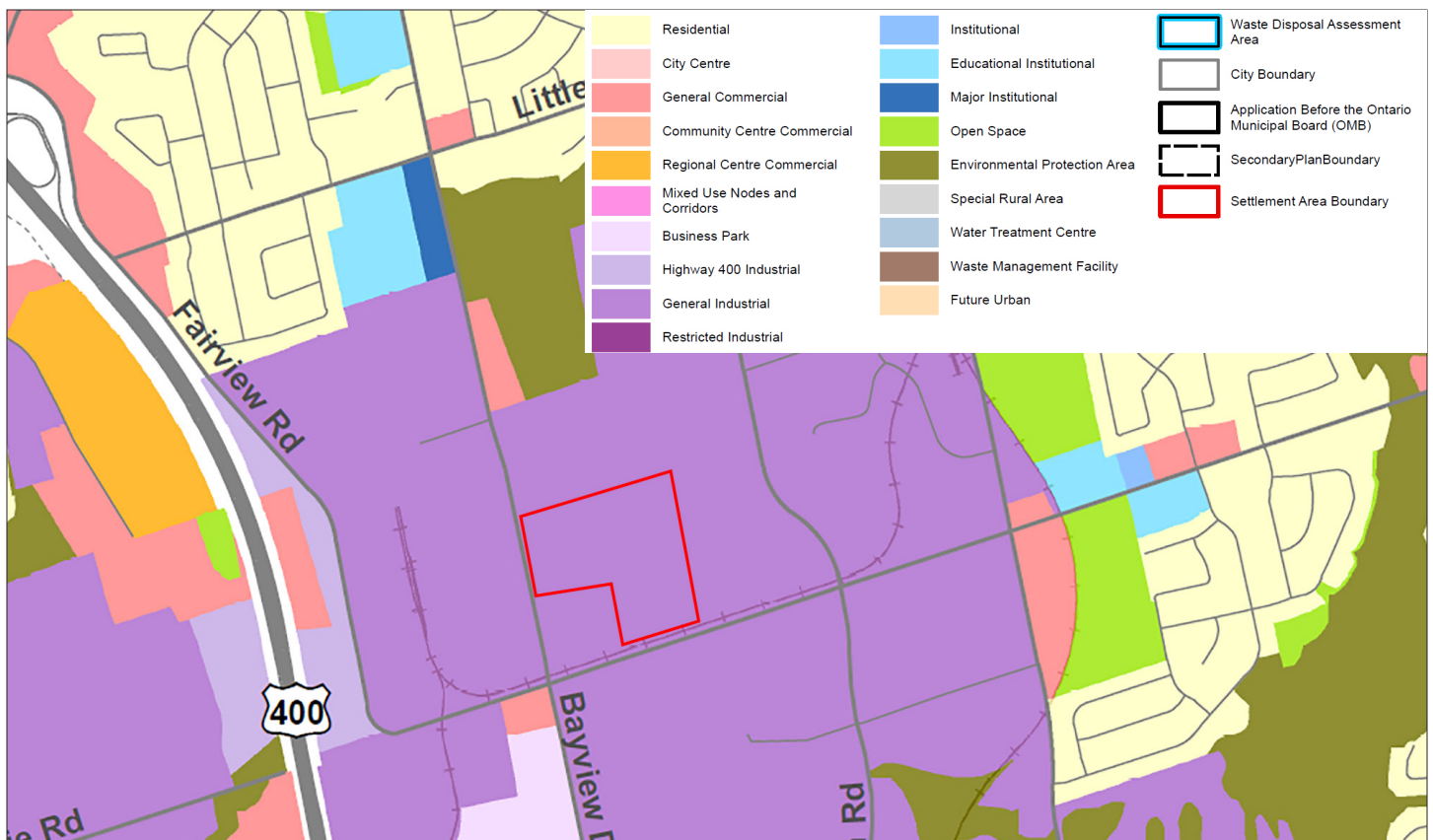


Figure 3. Official Plan - Schedule A Land Use

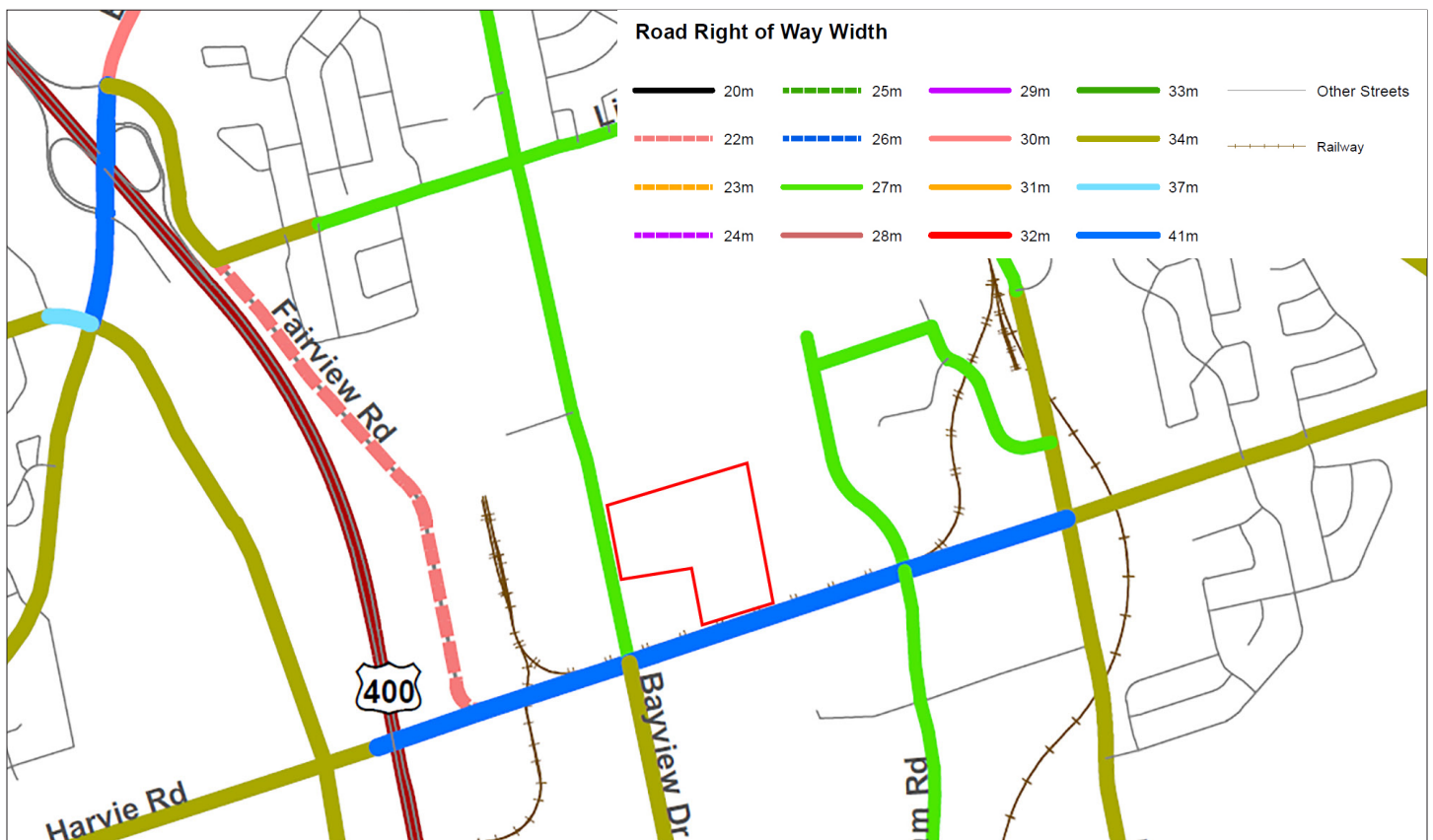
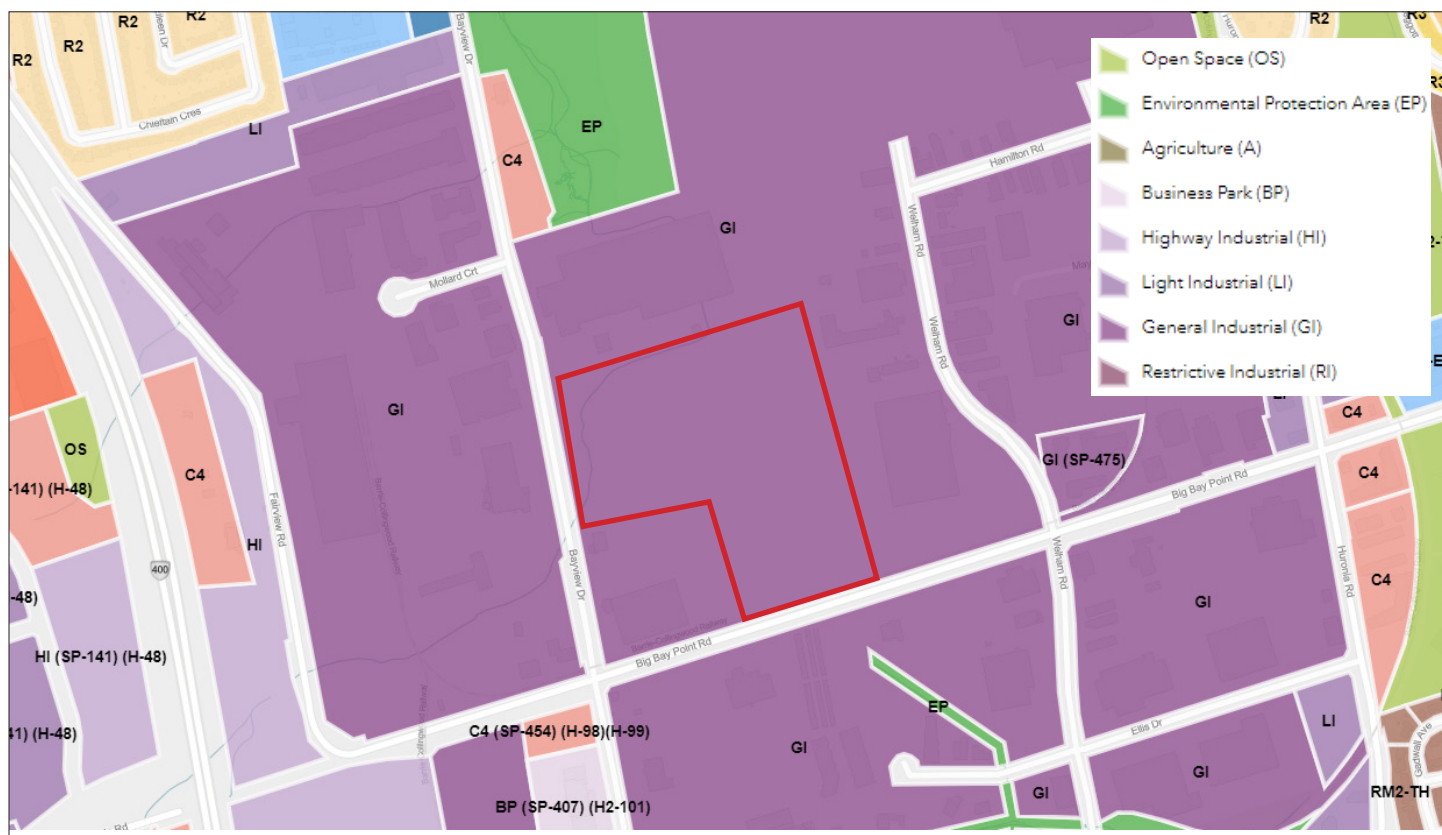
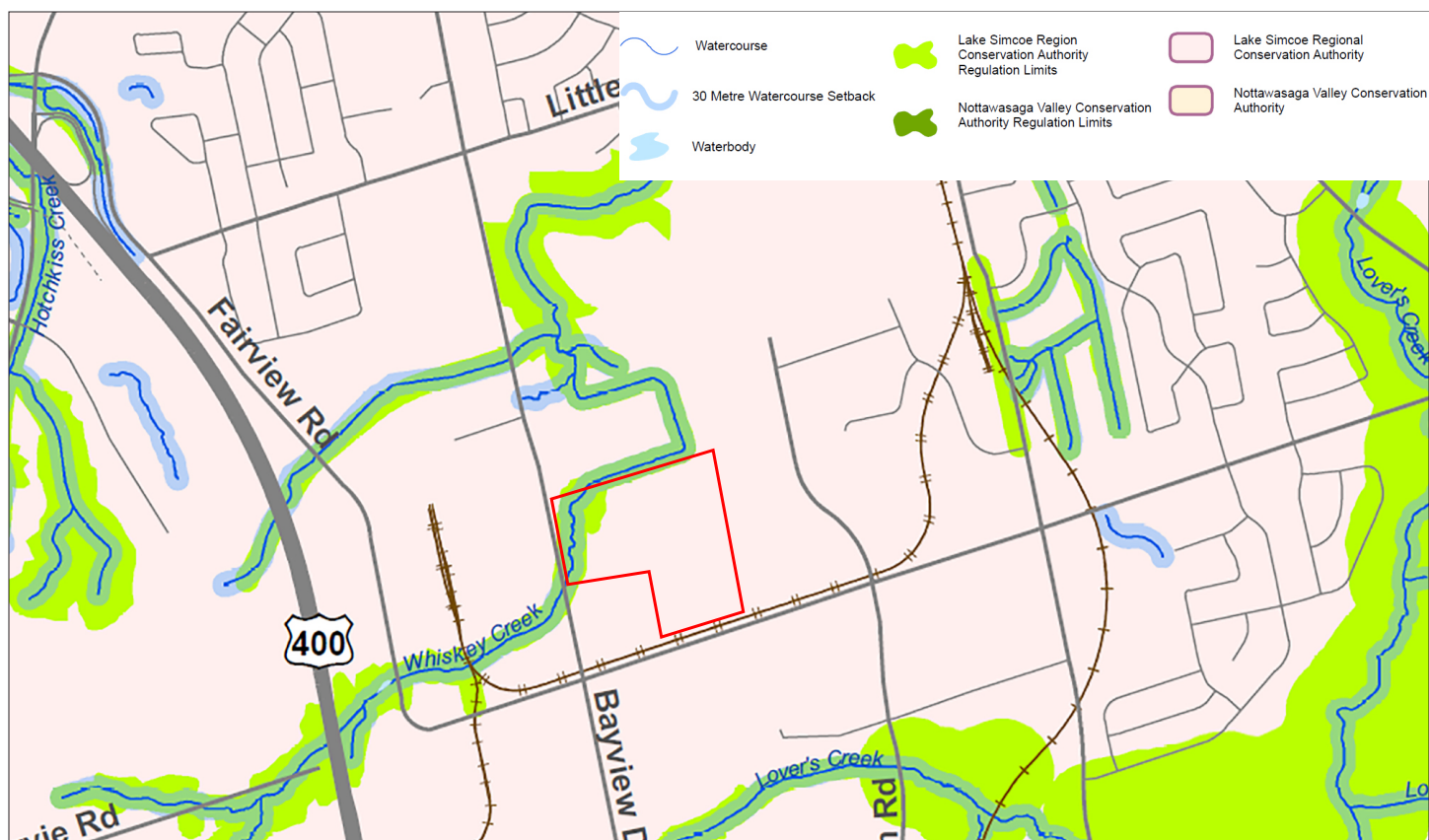


Figure 4. Official Plan - Schedule E Road Widening Plan



The industrial area surrounding the property can be considered a legacy industrial area in the City. However, several major infrastructure improvements are underway which will greatly improve access. Big Bay Point and Bayview Drive are both going through significant improvements. Big Bay Point is being widened to four (4) lanes along with Bayview Drive at the intersection with both roads. Big Bay Point proceeding west just recently completed the Harvie Road overpass over Highway 400 (located approximately 800 metres west of the subject site) which will greatly improve vehicle/pedestrian movements in the area. There does exist some large parcels of land south of Big Bay Point Road along the Highway 400 frontage that remain vacant and are designated industrial, commercial and residential uses.

Along the northern site boundary and along its western edge, closer to Bayview Drive, there is a watercourse and lands within the Lake Simcoe Region Conservation Authority Regulation Limits, as per Schedule F - Conservation Authority Regulation Limits and Watercourses. Schedule H of the Official Plan, Natural Heritage Resources, identifies much of the property as Level 1 with Existing Development Designation Subject to 3.5.2.4 d.

There are two Environmental Protection areas within the site's context. The nearest is Lackie's Bush located several hundred metres to the site's north. Lovers Creek Ravine is located in excess of 1.5 kilometres west of the subject site. Huronia Park is designated Open Space, and is located approximately 1 kilometre west of the site, on Huronia Road.

The subject lands and surrounding uses are illustrated in Figure 4.

North -	General Industrial, Environmental Protection, Small area of General Commercial
East -	General Industrial
South -	BRCY Rail Line, General Industrial
West -	General Industrial

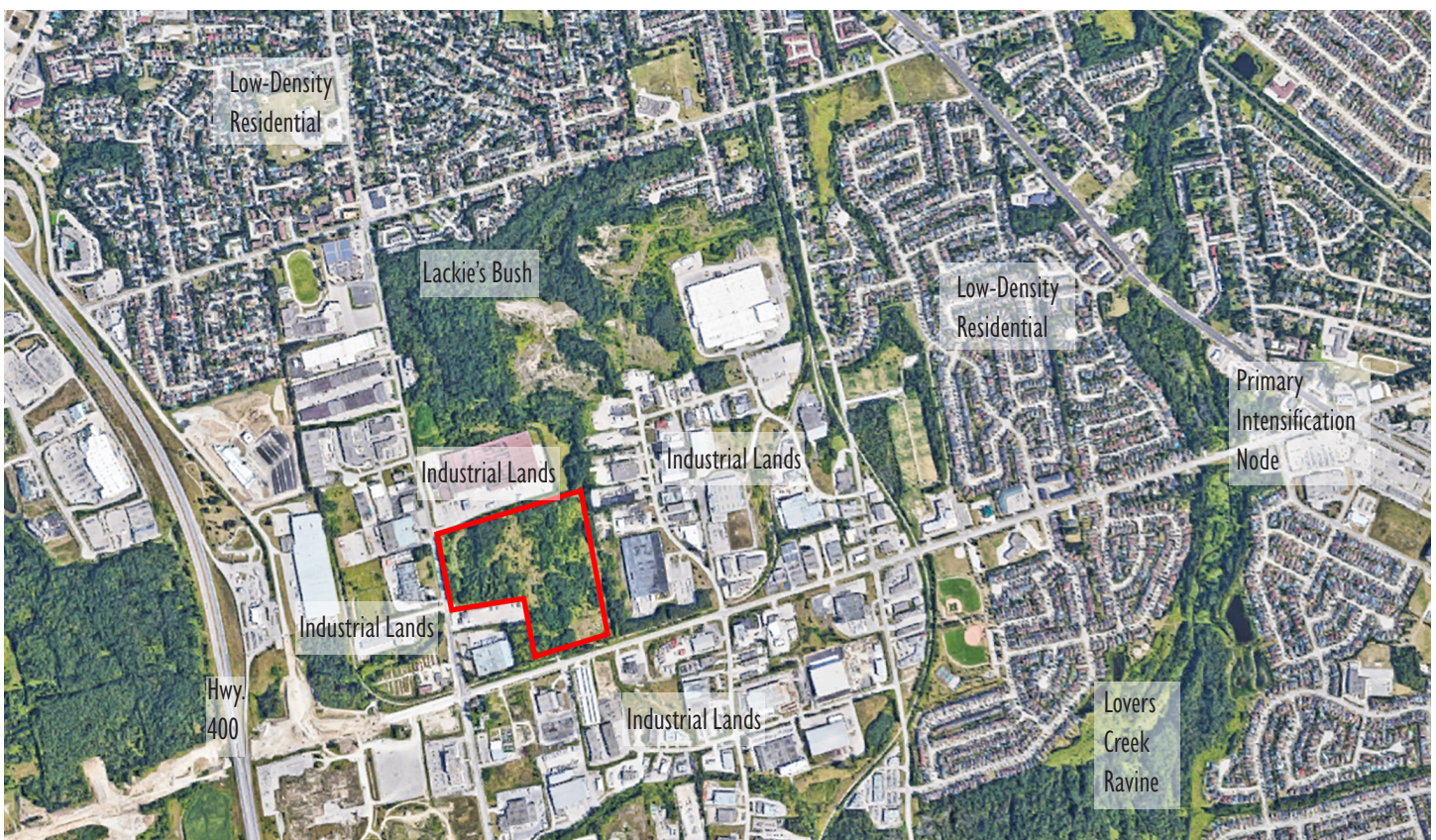


Image 4 - Surrounding Context



Image 5 - Industrial use west of site on Bayview Dr.



Image 6 - Looking East down Big Bay Point Road across from Subject Lands



Image 7 - Industrial use south of subject site.



Image 8 - Intersection of Bayview Dr. & Big Bay Point Road.



Image 9 - Industrial uses south of Big Bay Point Road.

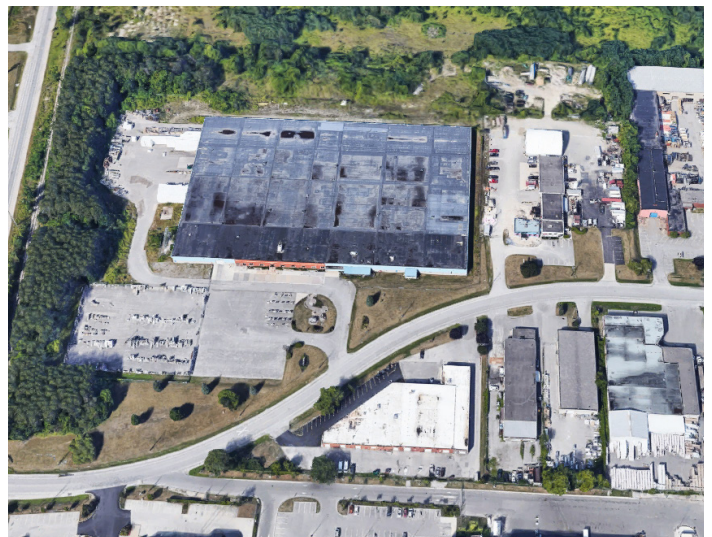


Image 10 - Industrial uses west of subject site.

DESCRIPTION OF THE PROPOSAL



3

3.1 Description of the Draft Plan of Subdivision

The draft plan proposes a new premiere industrial subdivision including a new 20m municipal right-of-way which extends east from Bayview Drive at the southern most section of the properties Bayview Drive frontage. The new right-of-way will be built to industrial standards and extends east into the site creating a loop within the interior of the subject property. The industrial subdivision will contain 31 industrial lots, 1 Environmental Protection Block, and 1 Stormwater Management Block. These blocks range in size and meet the required performance standards for the General Industrial Zoning. The lots are smaller in size compared to other industrial lots within the area but can be merged through the development process to create larger parcels for industrial users depending on market demand.

A 4.5 metre secondary emergency access point is provided at the southwest corner of the site and utilizes the existing railway crossing. It should be noted that the access along Bayview Drive has been designed to be larger road width to permit emergency services. Subject to review by Emergency Fire Services, the 4.5m secondary emergency access point is intended to be eliminated to facilitate more efficient use of the lot (lot 1).

An 8.0 metre servicing easement is located in the southeast corner of the property between lots 3 and 4 and will enable the site to be serviced from Big Bay Point Road with municipal water and sewage. Figure 7 illustrates the Draft Plan of Subdivision and it can also be found under Appendix 1.

As a whole, the blocks and lots will result in a comprehensive industrial community with the necessary provisions to accommodate the sites stormwater management needs, as well as the protection of sensitive environmental areas.

The total site area of the property is 15.6 h (156,000m²). A total of 31 industrial lots are proposed with each lot contemplated to accommodate buildings that will conform to the existing zoning standards (General Industrial (G1)). Individual lot designs are intended to be further refined through the building permit process of each lot owner.

It should be noted that the proposed draft plan of subdivision is contemplated to be completed in two phases. This is due to potential environmental constraints located on the phase two lands. The intention is to proceed with phase one while working through the environmental considerations on the Phase two lands.

Phase one will consist of the development of the right-of-way from Bayview Drive as well as the majority of the loop road which will temporarily terminate at a cul-de-sac in the northwestern portion of the property. Phase one will include the development of 24 industrial lots as well as the stormwater management pond block located in the most north eastern corner of the subject site. The proposed emergency access point located in the southwest corner of the property over the existing railway crossing will be utilized as temporary construction access to facilitate the development of phase one.

Phase two will consist of the development of the remainder of the loop road, the majority of the environmental protection block as well as 7 additional industrial lots. As mentioned earlier, the phase two lands are subject to environmental constraints including an existing watercourse which is currently being studied. A detailed Environmental Impact Study is being developed by Cambium Inc. for the phase two lands and will be submitted once completed. Development on the phase two lands is intended to occur once all of the environmental considerations have been addressed.

3.2 City of Barrie Official Plan

The subject lands are currently designated 'General Industrial' as per Schedule A Land Use in the Official Plan. Figure 3 above illustrates the Official Plan designation of the subject lands and lands in the immediate vicinity.

An Official Plan Amendment is not required for the application.

3.3 City of Barrie Zoning By-law 2009-141

As previously mentioned, the subject lands are currently zoned 'General Industrial (GI) Zone' as per the City of Barrie Zoning By-law 2009-141. Figure 8 below illustrates the current zoning of the subject lands and that of properties within the general vicinity.

Section 7.0 of the Zoning By-law provides the provisions for Industrial zones including 'General Industrial (GI) Zone'.

The uses permitted within the 'General Industrial (GI) Zone' include but are not limited to the following:

- Animal Shelter
- Bakery
- Concrete Product Manufacturing
- Foundry
- Manufacturing and Processing in Wholly Enclosed Buildings
- Material Recovery Facility (1)
- Cannabis Production Facility
- Outdoor Storage
- Printing and Publishing
- Pail Transfer Facility
- Recyclable Materials Transfer Station (1)
- Rental Store Excluding Video and Electronic Rentals

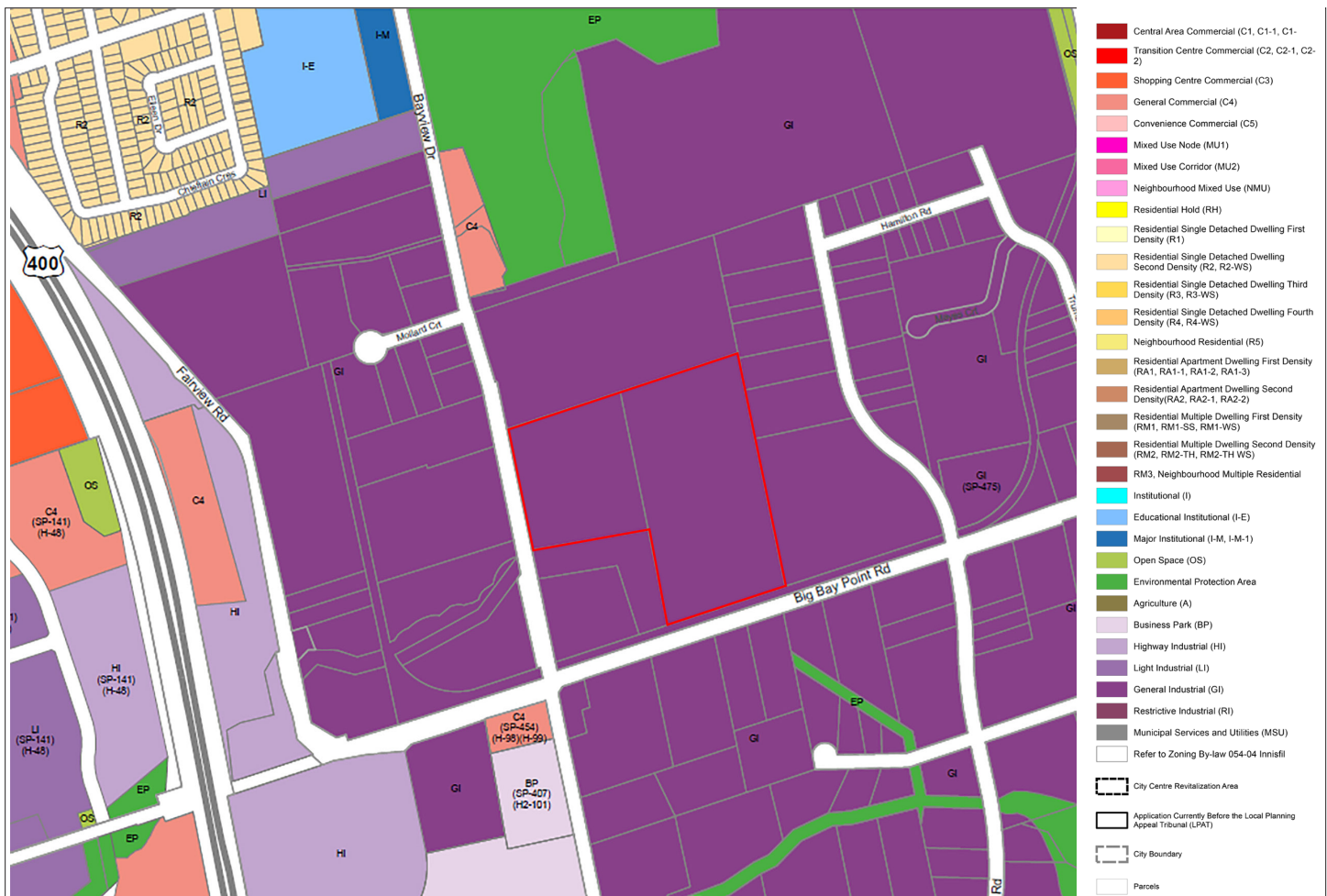


Figure 8. General Industrial (GI) Zone - Barrie Zoning By-law 2009-141

- Research/Development Facility
- Self Storage
- Truck Terminal
- Warehousing Establishment

Commercial uses:

- Accessory Employee Use
- Accessory Retail
- Adult Entertainment Parlour
- Automotive Repair Establishment
- Building Supply Centre (2)
- Car Wash
- Custom Workshop
- Data Processing Centre
- Drive Through Facility
- Dry Cleaning Establishment
- Golf Driving Range
- Nursery or Garden Supply Centre
- Office
- Outdoor Display and Sales Area
- Restaurant (3)
- Service Store
- Transmission Establishment, Cellular and Electronic
- Veterinary Clinic

Agricultural Uses

- Heavy Equipment Dealer
- Kennel

Institutional Uses

- Industrial School

(1) Shall be conducted only within a fully enclosed building. No outdoor storage or accessory outdoor storage will be permitted in association with these uses.

(2) Shall be restricted to a multi-tenanted building, to a maximum of 25% of the gross floor area of the total gross floor area of the building, to a maximum of 1000m², and that the provisions of Section 7.2.2 "Accessory Retail Uses" shall not apply. (By-law 2015-068)

(3) Shall only be permitted as part of a multi-tenanted building. (By-law 2019-115)

The uses permitted in the Industrial Zone are subject to the development standards referenced in Table 7.3 (Figure 9).

Table 7.3						
		Zones				
		Business Park (BP)	Highway Industrial (HI)	Light Industrial (LI)	General Industrial (GI)	Restricted Industrial (RI)
Lot Area (min.)		1300m ²	1300m ²	700m ²	700m ²	2000m ²
Lot Frontage (min.)		30m	30m	15m ⁽¹⁾	15m ⁽¹⁾	45m
Front yard (min.)		6m	15m ⁽²⁾	7m	7m ⁽²⁾	15m
Side Yard (min.)		4m	4m	3m	3m	3m
Side Yard Adjoining	Residential Zone (min.)	10m	10m	10m	10m	40m
	Street (min.)	6m	7m	7m	7m	7m
Rear Yard (min.)		6m	8m	7m	7m	7m
Rear Yard Adjoining	Residential Zone (min.)	15m	15m	15m	15m	40m
	Street (min.)	6m	15m	7m	7m	15m
Lot Coverage (max.)		50%	50%	60%	60%	60%
Building Height (max.)		14m ⁽³⁾	14m ⁽³⁾	14m ⁽³⁾	-- ⁽³⁾	-- ⁽³⁾

Figure 9. Industrial Standards Chart Excerpt from Zoning By-law 2009-141

- (1) Except in the case where an Industrial Zone abuts onto an arterial road as identified in the City of Barrie Official Plan in which case the lot frontage shall be increased to 30m.
- (2) The lot line and yard of any lot in any Highway Industrial (HI) Zone and/or General Industrial (GI) Zone which adjoins the right-of-way of Highway 400 shall be deemed to be the front lot line or front yard of the lot and the provisions of this By-law shall apply accordingly.
- (3) Except in the case where lands abut a Residential Zone in which case the maximum building height shall be 9m. Cranes, conveying equipment and incidental equipment required for manufacturing and processing shall also be exceptions to the maximum building height standard. (By-law 2015-068)

As noted previously, the end users of the new lots are unknown, therefore the lots have been laid out in conformance with the Zoning By-law Provisions in regard to minimum lot frontage and area.

3.4 Supporting Studies

A series of technical reports have been completed to ensure the following proposal is feasible. Reports that are included with the following application are as follows:

- Archaeological Study and Clearance
- Environmental Impact Study
- Traffic Impact Study
- Geotechnical Investigation Report
- Hydrogeological Investigation
- Functional Servicing and Preliminary Stormwater Management Report
- Tree Inventory and Preservation Plan Report

Archaeological Study and Clearance

A stage 1 & 2 Archaeological Assessment prepared by Irvin Heritage Inc. dated May 2021 was completed in support of the development application. A review of data within the provincial archaeological database indicated that a portion of the Study Area had been previously subject to Stage 1 and 2 Archaeological Assessments in 2007 and 2008 (Sutton 2007, 2008). The portion of the Study Area not subject to previous assessments was found to retain archaeological potential. As such, a Stage 2 Archaeological Assessment consisting of a 5 m Test Pit Survey was conducted. The Stage 2 identified no archaeological resources within the Study Area. The report concludes that the study area has been sufficiently assessed and is free of further archaeological concern.

Environmental Impact Statement

An Environmental Impact Study prepared by Cambium Inc. dated August 2021 was completed in support of the application and serves to address potential negative impacts to natural heritage features identified during the preliminary development review process, as required by the Provincial Policy Statement, Growth Plan for the Greater Golden Horseshoe, Lake Simcoe Protection Plan, and City of Barrie Official Plan. The study details the natural heritage features, identifies potential negative impacts to natural features, and recommends appropriate mitigation measures. The data presented in the EIS was collected through a review of background documents and seasonally appropriate field investigations. The data collected for the subject property was used to characterize the natural heritage features on the subject property, and was assessed against applicable policies in the City of Barrie Official Plan, and the guidelines and policies provided by regulatory agencies including the LSRC and the Ministry of Natural Resources and Forestry (MNRF).

The EIS concludes;

“In closing, potential negative impacts associated with the proposed development and site alteration can be appropriately minimized, provided that the recommendations summarized in Section 8.0 are adhered to. The information presented herein demonstrates that the proposed development can be carried out in a way that will not adversely impact natural heritage and hydrologic features and functions identified on or adjacent to the subject Site. Furthermore, the proposed development complies with applicable natural heritage policy.”

Functional Servicing and Preliminary Stormwater Management Report

PEARSON Engineering Ltd. was retained to prepare a Functional Servicing Report & Preliminary Stormwater Management (SWM) Report (FSR) in support of the proposed application located at 80 Big Bay Point Road (BBPR).

This FSR assessed the required municipal services to support development of the Project Lands, complete with preliminary grading, servicing and SWM design and associated calculations. The project will connect to a combination of existing and proposed municipal services located on Big Bay Point Road and Bayview Drive and a proposed SWM Pond at the northeast corner of the site.

Sanitary Servicing

An existing 300mm sanitary sewer stub is located at the southeast corner of the site and connects to the existing BBPR sanitary sewer. It is proposed to extend the sanitary sewer through an 8.0m wide service easement into the Project site.

The proposed 250mm sanitary sewer will convey flow to the existing sewer on BBPR and has a capacity of 42.06 L/s at 0.5%. The existing 300mm stub has 0.3% grade with a design capacity of 52.97 L/s. The proposed peak flow is approximately 31.8% of the pipe capacity therefore the existing 300 mm diameter sanitary sewer is sufficient to convey the sanitary design flows.

It is proposed that the sanitary sewers be constructed in accordance with the City of Barrie and the MOE guidelines to service the Project. The proposed sewers will consist of a minimum diameter of 250mm and will be designed to meet minimum design grades and the required minimum and maximum velocities under flow conditions.

Water Supply and Distribution

The Project Lands are proposed to be serviced by connecting to existing 300mm watermain located on the west side of Bayview Drive and the north

side of BBPR. A proposed 200mm watermain will connect to the Bayview Drive watermain and extend through the Project site within the proposed road allowances and through a service and emergency access to the BBPR watermain.

Each proposed industrial lot will receive a domestic and fire water service. Internal fire hydrants will be proposed to provide adequate firefighting coverage as per City Standards. The available flow meets the required fire flow as per the City of Barrie requirements.

Drainage and Stormwater Management

The Project Lands are mostly pasture lands and primarily drain north toward the Whiskey Creek with a small portion of the site draining south through a culvert under the existing rail lines into the Lover's Creek watershed to the south of BBPR. Whiskey Creek north and around the side of The Source property north of the Project Lands.

The post development drainage for the site will generally follow the split drainage pre-development drainage patterns. The proposed storm sewer has been designed to convey the runoff from the Industrial blocks and internal road. The Project's storm sewer will be sized for the minor storm, defined as all storms up to and including the 5-year storm event. In the event of a storm event greater than the 5-year storm, the storm sewer will overflow, and the water will be conveyed overland towards the proposed SWM Pond. The SWM Pond which will provide both quality and quantity controls up to the allowable runoff coefficient.

The following recommendations shall be implemented and maintained during construction to achieve acceptable stormwater runoff quality:

- Installation of silt fence along the entire perimeter of the site to reduce sediment migration onto surrounding properties.
- Installation of a construction entrance mat at the entrance to minimize transportation of sediment onto roadways.
- Restoration of exposed surfaces with vegetative and non-vegetative material as soon as construction schedules permit;
- Installation of filter strips where applicable.
- Reduce stormwater drainage velocities where possible;
- Ensure that disturbed areas are vegetated and stabilized as quickly as possible;

Water Balance

Infiltration options will be reviewed at detailed design. It is required to provide retention for the first 5 mm of rainfall over the site area as per City of Barrie Guidelines.

Phosphorous Calculations

To minimize the amount of phosphorus discharged from the site, a treatment train approach is to be utilized. Full approach and detailed calculations will be completed with detailed design.

Grading

A preliminary grading design has been completed for the project to confirm drainage of the site. The grading has been designed to generally flow to the north east corner of the property at an average grade of 0.5%, allowing the majority of the site to be conveyed to the proposed SWM pond.

Secondary Utilities

Consultation with existing Utility companies is currently underway to confirm the serviceability for secondary utilities. Based on the expanding development and upgrades currently underway for both Big Bay Point Road and Bayview Drive it is expected that there is available capacity with little to no upgrades to the existing systems

Hydrogeological Investigation

Cambium Inc. was retained by the client to complete a hydrogeological assessment of the property located at 80 Big Bay Point Road & 315 Bayview Drive, City of Barrie, Ontario.

The hydrogeological investigation was carried out with the following tasks:

- Review of available background information: a review of available geological and hydrogeological information for the site and surrounding areas and the previous investigation reports completed for the Site, was conducted to provide background information to allow for characterization of the Site's soil and groundwater conditions.
- Detailed site inspection: an inspection of the Site was completed to review existing site conditions including identification of any hydrogeological features such as significant areas of potential groundwater recharge or areas of groundwater discharge.
- Measurement of groundwater levels: groundwater levels were measured in the existing monitoring wells to establish and/or confirm the general groundwater flow condition.
- Soil Infiltration Tests: will be completed at selected locations using Guelph Permeametre in order to determine the soil infiltration characteristics for the design and construction of Low Impact Development (LID) measures.

- **Water Balance (Preliminary):** a preliminary water balance study was completed for the proposed development using the Thornthwaite-Mather approach and the climatic data obtained from Environment Canada.
- **Report Preparation:** a hydrogeological report was prepared presenting the results, findings, and recommendations of this investigation.

Groundwater levels were determined to range from as shallow as 0.54 mbgs to as deep as 4.30 mbgs and groundwater flow was estimated to be towards west and northwest. As the proposed development will be finished as slab-on-grade and therefore not expected to be significant dewatering efforts required for construction or operation of the proposed development. However, the proposed development will result in an infiltration deficit at the Site. The infiltration deficit can be accounted for if the runoff from roof surfaces (or a portion thereof) is captured and re-infiltrated at the Site.

Further, there were some regulated areas mapped on-site that could potentially be influenced by the proposed development and therefore, regulation area development restrictions shall apply to the proposed development.

Adjacent landowners/properties are on City's water supply and therefore are not anticipated to be influenced from the proposed development. However detailed development plans should be reviewed (once prepared) to determine if dewatering is required during construction/operation of the development, and the potential influence of dewatering activities, if any. In addition, the water balance should also be reviewed when more detailed development plans with landscape areas are available.

Preliminary Geotechnical Investigation

Cambium Inc. was retained by the client to complete a geotechnical investigation in support of the proposed application.

Physical laboratory testing, including five sieve and hydrometer analyses was completed on selected soil samples to confirm textural classification and to assess geotechnical parameters. Natural moisture content testing was completed on all retrieved soil samples.

Based on the results of the borehole investigation, the subsurface conditions at the site consist of a surficial layer of topsoil overlying brown sand material, which overlies a brown sandy clayey silt to silty clay layer in some of the borehole locations to the termination depths of 6.6 mbgs. The boreholes were terminated in native soils and bedrock was not encountered within the investigation depths.

The moisture content of the soils generally ranged from 4% to 47%. It should be noted that soil moisture and groundwater levels at the Site may fluctuate seasonally and in response to climatic events.

It is assumed that all the proposed lots will be developed with slab on grade structures and municipally serviced. Based on site observations the existing elevations of the surrounding roadways and boreholes, significant grade raise is not anticipated at this site. It is anticipated that most of the excavations will be above the ground water elevation based on the groundwater measurements during our investigation.

Cambium should be retained to complete testing and inspections during construction operations to examine and approve subgrade conditions, placement and compaction of fill materials, granular base courses, and asphaltic concrete.

Traffic Impact Study

A Traffic Impact Study Report was completed by GHD dated August 2021 and establishes the existing and future road network and the subsequent traffic-related impacts of the subject site at the study intersections during the weekday a.m. and p.m. peak hours. These impacts are based on projected future background traffic derived for build-out in 2024 and a period of five and ten year post build-out in 2029 and 2034.

The report concludes that:

"The overall impact of the proposed development is minor and will not adversely impact the operation of intersections Big Bay Point Road, Bayview Drive, Fairview Road, and Little Avenue. With the signal optimization applied to the 2024, 2029, and 2034 future conditions, key critical movements can be kept to below their theoretical maximum capacities."

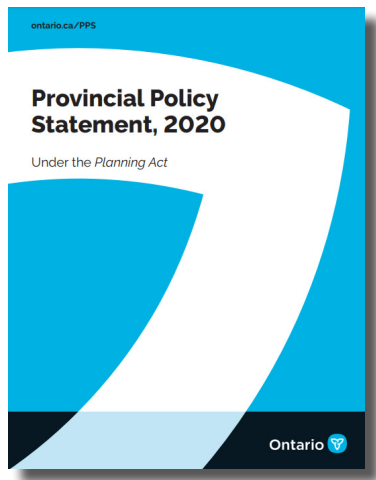
GHD has also found the proposed configuration and location of the site access to be acceptable, with no expected operational or safety concerns for Bayview Drive."

PLANNING ANALYSIS



4

This Section will outline the applicable planning and development policies impacting this application. Each sub-section will outline the policies and provide a planning analysis with respect to conformity of this application.



4.1 Provincial Policy Statement (2020)

The Provincial Policy Statement (2020) (PPS) has been reviewed relative to this proposal with specific attention paid to:

- Section 1.1 Managing and Directing Land Use
- Section 1.1.3 Settlement Areas
- Section 1.3 Employment Areas
- Section 1.6 Infrastructure and Public Service Facilities
- Section 1.7 Long-Term Economic Prosperity
- Section 2.1 Natural Heritage

The proposed development is within the Settlement Area of the City of Barrie. Section 1.1.1 of the PPS outlines policies which sustain a healthy, liveable and safe communities. The proposed development is consistent with these policies, specifically:

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;
- b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;
- d) avoiding development and land use patterns that would prevent

the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;

- e) promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;
- f) improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;
- g) ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs; and
- h) promoting development and land use patterns that conserve biodiversity

The application proposes a development which supports and sustains economic development and growth within Barrie, through the development of 31 industrial lots which will provide jobs to the surrounding area on lands that are within the settlement area and designated for industrial development. The proposal makes efficient use of the subject lands while maintaining and protecting the natural environmental features which run along the northern and western portion of the property. The proposal will utilize the existing infrastructure within the area, while building upon these services through the creation of a new public Street 'A'.

Section 1.1.3 of the PPS states that settlement areas will be the focus of growth and development. The vitality and regeneration of settlement areas is critical to the longer-term economic prosperity of our communities. Land use patterns are to be based on densities and a mix of land uses which:

- a) efficiently use land and resources;
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;
- c) minimize negative impacts to air quality and climate change, and promote energy efficiency;
- d) prepare for the impacts of a changing climate;
- e) support active transportation;
- f) are transit-supportive, where transit is planned, exists or may be developed; and
- g) are freight-supportive.

The subject lands are designated for industrial development and are surrounded by industrial and employment uses. The lands are to be divided in a way that is efficient, extending the necessary infrastructure to each subdivision lot. The subject lands are well connected to the surrounding road network which will support the movement of goods.

Section 1.3 outlines policies related to Employment and Employment Areas. Economic development and competitiveness is to be promoted by:

- a) providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;
- b) providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;
- c) facilitating the conditions for economic investment by identifying strategic sites for investment, monitoring the availability and suitability of employment sites, including market-ready sites, and seeking to address potential barriers to investment;
- d) encouraging compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities, with consideration of housing policy 1.4; and
- e) ensuring the necessary infrastructure is provided to support current and projected needs.

The proposed development provides 31 lots within an existing industrial area. It provides the opportunity to create new employment uses contributing to the mix and range of use, which diversify the economic base providing land for future job creation and business opportunities. The lands are located in a strategic area within an existing larger established industrial/employment area and are well connected to the surrounding road network. In addition, the subject lands have adequate separation from sensitive land uses, such as residential uses, therefore ensuring the long-term operational and economic viability of the uses and area.

Section 1.6 provides policy related to Infrastructure and Public Service Facilities, which are to be provided in an efficient manner.

Section 1.6.6 relates specifically to sewage, water and stormwater. Municipal sewage services and water services are the preferred form of servicing settlement areas. Municipal services are proposed to be taken from Big Bay Point Road, and enter the site via an 8.0 metre easement located in the southeast corner of the subject property. Policies related to stormwater management is provided which ensures that systems are optimized, feasible and financially viable over the long term, minimize contaminant loads, minimize erosion and changes in water balance, maximize the extent and

function of vegetative and pervious surfaces, and promote best practices, including stormwater attenuation and re-use, water conservation and efficiency, and low impact development. A Functional Servicing and Preliminary Stormwater Management Report, prepared by Pearson Engineering, dated August 2021 has been included with this submission, which provides the stormwater management plan for the subject lands.

Section 1.6.8 relates to Transportation and Infrastructure Corridors, which are to be planned and protected for the long term. The proposed development is adjacent to an existing rail corridor. The proposed development does not hinder the ability to utilize this corridor, rather it strengthens the future use of the rail corridor for land users who may wish to use it as a form of transportation.

Section 1.7 relates to Long-Term Economic Prosperity, which should be supported. The proposed development contributes to economic development within the City of Barrie. It creates 31 lots and 2 blocks, which promote the opportunity for future economic development and investment-readiness. The lands provide a variety of opportunity for general industrial uses, utilizing the land, resources, infrastructure and public service facilities.

Section 2.1 relates to Natural Heritage features, which are to be protected for the long term. An Environmental Impact Study, prepared by Cambium, dated August 2021 has been submitted in support of this application.

Section 2.1 of the Provincial Policy Statement (PPS) protects the form and function of natural heritage features as defined by the PPS. Natural heritage features included in the PPS are provincially significant wetlands (PSW), significant coastal wetlands, significant woodlands, significant valley lands, significant wildlife habitat (SWH), significant areas of natural and scientific interest (ANSI), fish habitat, and the habitat of endangered and threatened species. Development in the habitat of endangered and threatened species shall only be permitted in accordance with provincial and federal requirements. Development within other natural heritage features and on lands adjacent to all natural heritage features are permitted only if demonstrated that there will be no negative impacts on the feature or their ecological function. Development includes the creation of a new lot, a change in land use, or the construction of buildings and structures requiring approval under the Planning Act.

Section 2.2 of the PPS protects the quality and quantity of water, including the form and hydrologic function of sensitive surface water features and sensitive ground water features. Focus is given to maintaining hydrologic linkages and functions at the watershed scale to minimize potential negative impacts, including cross-jurisdictional and cross-watershed impacts of development. Mitigative measures and/or alternative development approaches should be considered for development near water features.

Based on the key natural heritage and hydrologic features and the findings of the field investigations conducted on the Site which are detailed within the Environmental Impact Study prepared by Cambium, the proposed development meets associated policy and the “no negative impact” policy test of the PPS applies. Refer to the enclosed Study for further review and details regarding the evaluation of the subject lands.

Based on the above, the proposed development is consistent with the Provincial Policy Statement and supports economic development and growth within the City of Barrie.

4.2 Places to Grow - Growth Plan for the Greater Golden Horseshoe (2020)



The Growth Plan for the Greater Golden Horseshoe has been prepared and approved under the Places to Grow Act. The Government of Ontario recognizes that in order to accommodate future population growth, support economic prosperity and achieve a high quality of life for residents of Ontario, planning must occur in a rational and strategic way. The Growth Plan, has been reviewed in conjunction with this application with emphasis placed on the following;

Section 1.2.1 Guiding Principles

Section 2.2.1 Managing Growth

Section 2.2.5 Employment

Section 3.2.7 Stormwater Management

Section 4.2.2 Natural Heritage System

Section 4.2.3 Key hydrologic features, key hydrologic areas and key natural heritage features

Section 4.2.4 Lands adjacent to key hydrologic features and key natural heritage features

Section 6 Simcoe Sub-area

The Growth Plan is based on several guiding principles listed in Section 1.2.1. The most applicable of these with respect to the subject applications are:

- Support the achievement of complete communities that are designed to support healthy and active living and meet people's needs for daily living throughout an entire lifetime.
- Provide flexibility to capitalize on new economic and employment opportunities as they emerge, while providing certainty for

traditional industries, including resource-based sectors.

- Protect and enhance natural heritage, hydrologic, and landform systems, features and functions.

The proposed application provides additional lands for economic growth and employment opportunities, while protecting the natural features within the subject lands. The applications provide the necessary services to these lands to facilitate this growth which in turn supports the achievement of complete communities.

Section 2.2.1 provides policies related to managing growth, providing population and employment forecasts for municipalities. Growth is to be directed to settlement areas which have existing or planned infrastructure and can support the achievement of complete communities.

"Complete communities" is defined as:

"Places such as mixed-use neighbourhoods or other areas within cities, towns and settlement areas that offer and support opportunities for people of all ages and abilities to conveniently access most of the necessities for daily living, including an appropriate mix of jobs, local stores, and services, a full range of housing, transportation options and public service facilities. Complete communities are age-friendly and may take different shapes and forms appropriate to their contexts."

The proposed development contributes to the City of Barrie being a complete community, providing diverse mix of land use within the City while contributing to the employment base.

Section 2.2.5 relates to Employment, and economic development and competitiveness within the Greater Golden Horseshoe (GGH). Employment area is defined as, "areas designated in an official plan for clusters of business and economic activities including, but not limited to, manufacturing, warehousing, offices, and associated retail and ancillary facilities."

Economic development is to be supported by ensuring the availability of sufficient land, in appropriate locations, for a variety of employment to accommodate forecasted employment growth. Lands located adjacent to or near major goods movement facilities and corridors, are to be areas for manufacturing, warehousing and logistics, and appropriate associated uses and facilities.

Municipalities are to establish minimum density targets for all employment areas within settlement areas which reflect the current and anticipated type and scale of employment that characterizes the employment area.

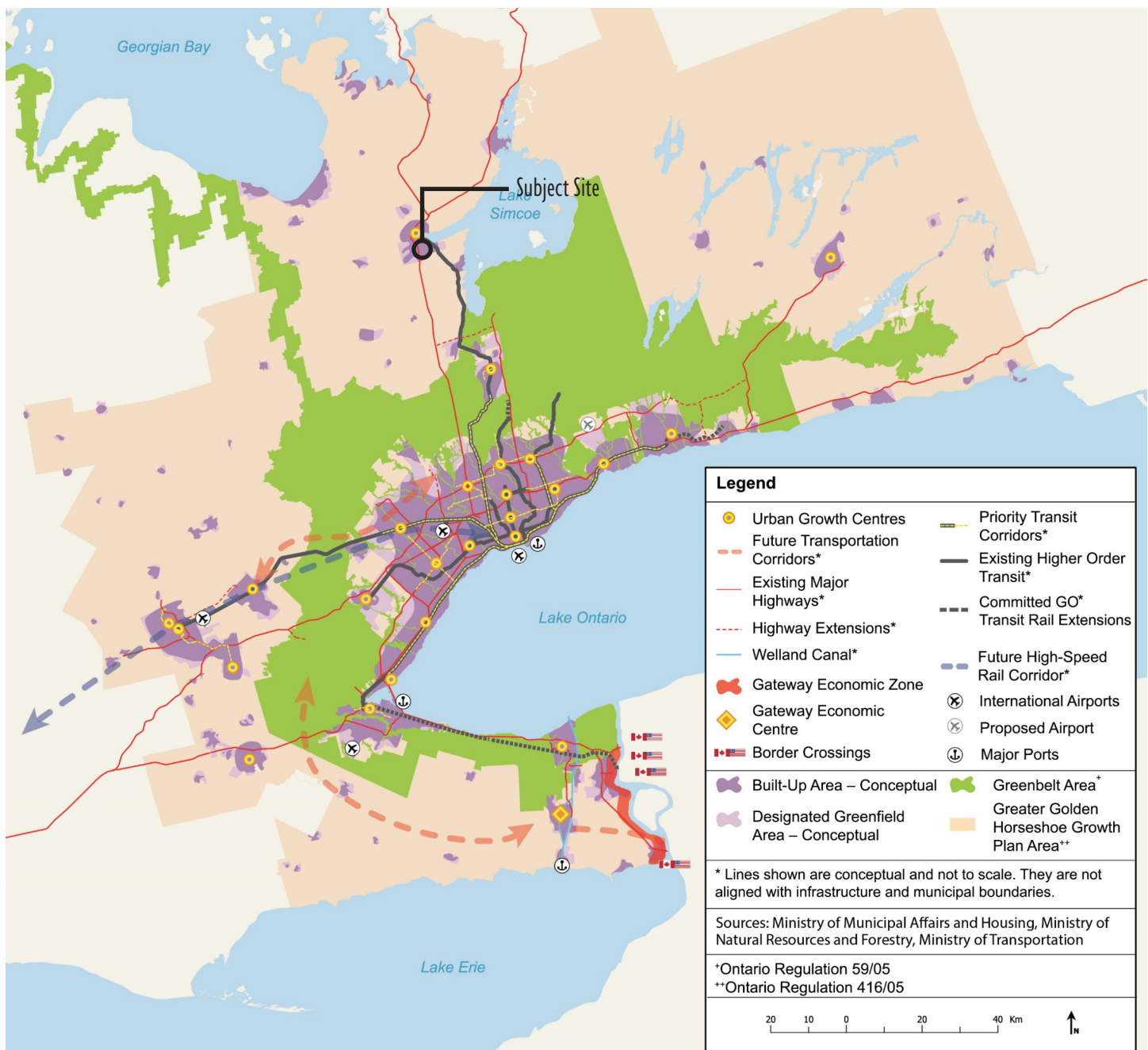


Figure 10. Schedule 2 - A Place to Grow Concept

The subject lands are designated for industrial use and are within an industrial/employment area providing a wide range of economic activities including manufacturing, warehousing, offices, and associated retail and ancillary facilities. The applications propose to provide serviced lands available for economic development which will accommodate forecasted employment growth. The subject lands are located adjacent to major movement corridors which are necessary to support industrial and employment uses.

Section 3.2.7 provides policies related to Stormwater Management. Municipalities are to develop stormwater master plans for settlement areas. Section 3.2.7.2 states, "Proposals for large-scale *development* proceeding by way of a secondary plan, plan of subdivision, vacant land plan of

condominium or site plan will be supported by a *stormwater management plan* or equivalent, that:

- is informed by a *sub watershed plan* or equivalent;
- incorporates an integrated treatment approach to minimize stormwater flows and reliance on stormwater ponds, which includes appropriate *low impact development* and *green infrastructure*;
- establishes planning, design, and construction practices to minimize vegetation removal, grading and soil compaction, sediment erosion, and impervious surfaces; and
- aligns with the *stormwater master plan* or equivalent for the *settlement area*, where applicable."

A Functional Servicing and Preliminary Stormwater Management Report, prepared by Pearson Engineering, dated August 2021 has been submitted in support of this application. This Report has reviewed the above and other applicable policies within the design of the stormwater system for the subject lands.

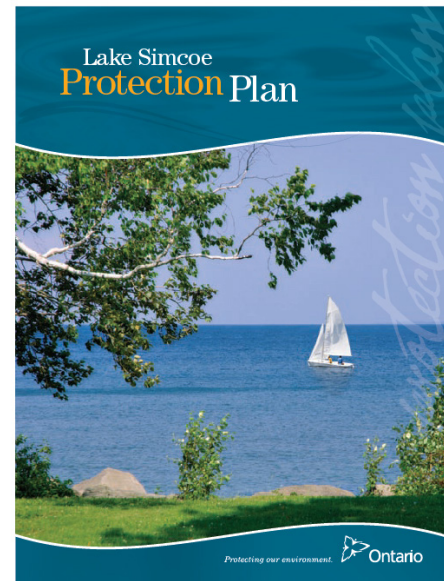
Section 4.2 provides policies for protecting what is valuable. Section 4.2.2 speaks directly to the Natural Heritage System. This system has been mapped by the Province to support a comprehensive, integrate and long-term approach to planning for the protection of the region's natural heritage and biodiversity. The Natural Heritage System for the Growth Plan excludes lands within settlement area boundaries. Municipalities are to protect any natural heritage features and systems in a manner that is consistent with the PPS, including those lands within settlement areas.

As noted within the Phase 1 Environmental Impact Study, prepared by Cambium, dated August 2021, the site is located within the City's settlement area; as such, the more restrictive natural heritage policies of the Growth Plan do not apply; however, natural features must be protected in a manner that is consistent with the PPS.

Section 6 of the Growth Plan provides policy direction specifically to the Simcoe Sub-area which includes municipalities within the County of Simcoe and the cities of Barrie and Orillia, where significant growth has been directed. The City of Barrie is identified as a Primary Settlement area on Schedule 8 of the Plan. Therefore, it will be an area that experiences strategic growth, which is to support the achievement of complete communities and ensure that development is of high-quality urban form through site design and urban design standards that create attractive and vibrant places that support walking and cycling for everyday activities and are transit-supportive. The proposed development contributes to the growth within Barrie and the achievement of a complete community through the creation of industrial/employment lands support strategic economic growth.

Based on the above, the proposed development conforms with the policies of the Growth Plan for the Greater Golden Horseshoe.

4.3 Lake Simcoe Protection Plan



The Lake Simcoe Protection Plan (LSPP) was established in 2009, in response to the decline in the ecological integrity of Lake Simcoe. The LSPP applies to the Lake Simcoe watershed and focuses on issues such as aquatic life, water quality and quantity, ecosystem health at the watershed scale, invasive species, climate change, and recreational activities. The LSPP includes policies with respect to the Lake Simcoe shoreline, key natural heritage features (KNHF), and key hydrologic features (KHF). KNHF include wetlands, significant woodlands, significant valley lands, and natural areas abutting Lake Simcoe. KHF include wetlands, permanent and intermittent streams, and lakes other than Simcoe.

With respect to development and site alteration proposals, policies 6.1 – 6.3, 6.5, 6.11 and 6.20 – 6.29 of the LSPP provide enhanced protections for KNHF and KHF; however, policies 6.32 – 6.34 apply to *existing settlement areas* and provide exemptions to the above policies, as follows:

6.32-DP Policies 6.32 - 6.34 apply to *existing settlement areas* and areas of Lake Simcoe adjacent to these lands, including the *littoral zone*, and these areas are not subject to policies 6.1 – 6.3, 6.5, 6.11 and policies 6.20 - 6.29.

6.33-DP An application for development or site alteration shall, where applicable:

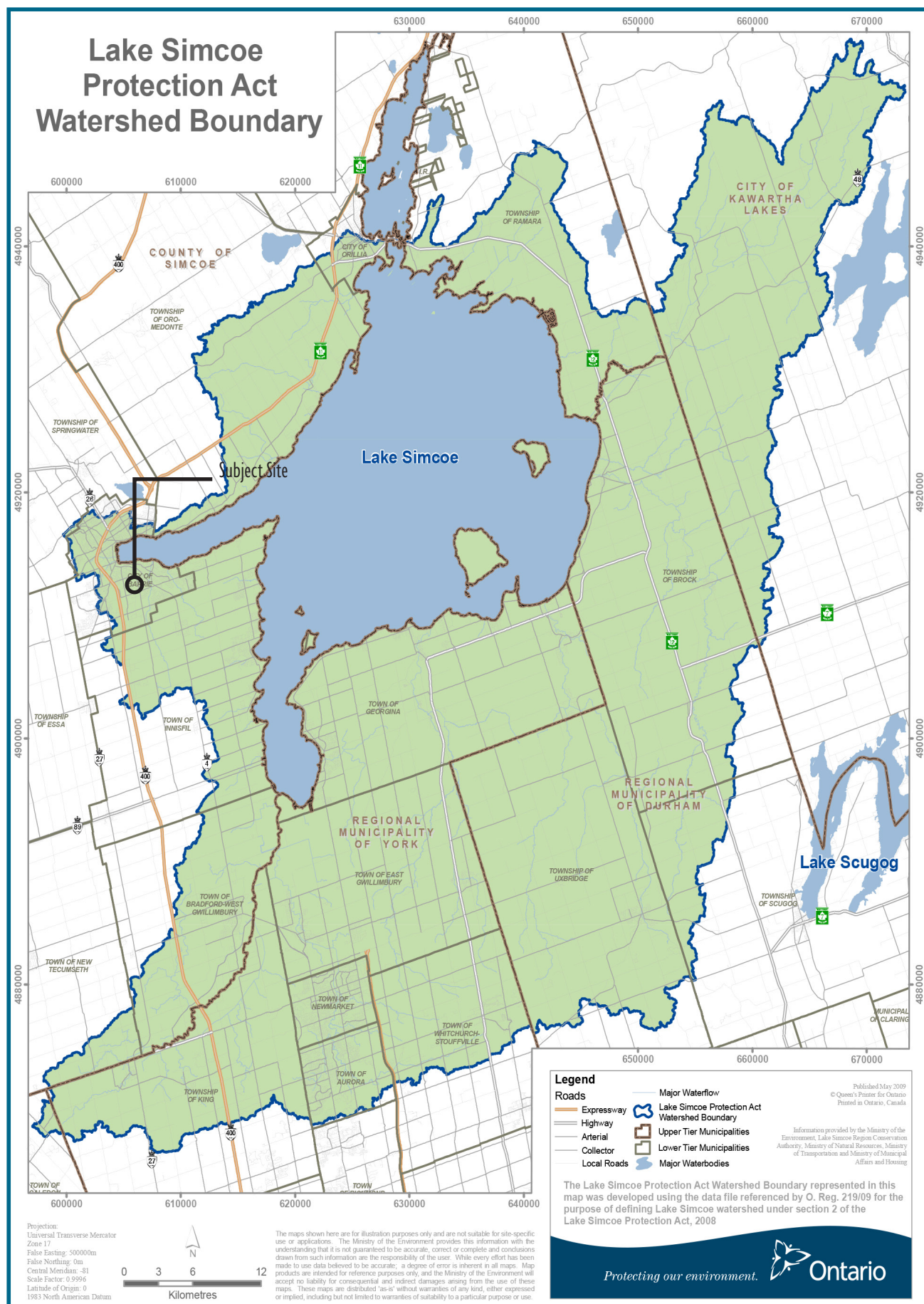


Figure 11. Lake Simcoe Protection Act Watershed Boundary

- a. increase or improve fish habitat in streams, lakes and wetlands, and any adjacent riparian areas;
- b. include landscaping and habitat restoration that increase the ability of native plants and animals to use valley lands or riparian areas as wildlife habitat and movement corridors;
- c. seek to avoid, minimize and/or mitigate impacts associated with the quality and quantity of urban run-off into receiving streams, lakes and wetlands; and
- d. establish or increase the extent and width of a vegetation protection zone adjacent to Lake Simcoe to a minimum of 30 metres where feasible.

6.34-DP Where, through an application for development or site alteration, a buffer is required to be established as a result of the application of the PPS, the buffer shall be composed of and maintained as natural self-sustaining vegetation.

The Site is located within the City's settlement area and a watercourse and wetlands are present on the subject lands. The Environmental Impact Study, prepared by Cambium, dated August 2021 has considered these policies within their Study of the subject lands. Based on the findings and recommendations of the EIS including the maintenance of vegetation protection zones or 30 metre buffer, will effectively mitigate potential impacts to fish and watercourse identified on the Site. Any potential impacts resulting from new built features of site alteration should be sufficiently mitigated through the implementation of the recommendations within the report, therefore the intent of these policies is maintained and satisfied.

4.4 City of Barrie Official Plan



As noted, the subject lands are currently designated 'General Industrial' as per Schedule 'A' Land Use in the Official Plan.

To facilitate the proposed development, an Draft Plan of Subdivision is required, to permit 31 industrial lots, 2 Blocks, and Street 'A'.

General Policies — Growth Management

Section 3.1 provides policies to guide growth and development within the City of Barrie, with an overarching goal of achieving a cost-efficient and economically sustainable, complete community. Accommodating the projected needs for employment lands is required to achieve a complete community with an appropriate mix of jobs and local services. Growth is to occur in a balance between employment areas and residential lands for development in order to achieve and maintain a ratio of approximately 1 job in the City for every 2 residents. The proposed development will contribute to achieving this target through appropriate development and subdivision of the subject lands in alignment with policies to provide employment lands to provide economic growth and sustainability, contributing to a complete community.

General Policies — Natural Heritage, Natural Hazards and Resources

Section 3.5 provides policies to identify, protect and enhance natural heritage features and areas. The policies note that new development shall be directed to maintain the natural landscape that shapes and defines the City's landform features, natural watershed drainage patterns and vistas.

Section 3.5.2.4 relates to Natural Heritage Resources within the City as depicted on Schedule H and identifies the site as containing areas with the following Natural Heritage Resource classifications; Level 1 with Existing Development Designation Subject to 3.5.2.4 d (see figure 12).

Policy 3.5.2.4 d states:

Notwithstanding the land use limitations applicable to properties identified as Level 1 in Section 3.5.2.4 (a) i), where an existing designation permits other forms of development, such development may proceed subject to the policies of Level 2 in Section 3.5.2.4 (a) ii) and the appropriate planning application processes.

Level 2

Level 2 resources represent significant components of the Natural Heritage Resource network. The features and function of these areas should be retained, however, there is potential for development if no negative impact can be demonstrated or mitigated. Level 2 resources include:

- Significant valley lands;
- Provincially significant life science ANSI;
- Significant wildlife habitat, including but not limited to core winter deer yards, colonial water-bird nesting sites, rare vegetation communities (i.e. tall grass prairies), and significant areas of vernal pools;
- Watercourse, minimum vegetation protections zones, and connectivity linkages;
- Woodlands greater than 4 hectares and less than 10 hectares.

Section 3.5.2.4 (a) ii) states that an EIS will be required to be completed for any development or site alteration in or within 120 metres of an area identified as Level 2 on Schedule H.

Based on the policy noted above an Environmental Impact Study prepared by Cambium, dated August 2021, has been submitted in support of the application (phase one only). The study addresses potential impacts to protected features identified on and adjacent to the Site that may result from the proposed development and site alterations. Wetlands, fish habitat/intermittent watercourse, and habitat of endangered and threatened species

were identified. No other natural heritage features protected by provincial policy were identified on or adjacent to the site. Mitigation and best practices have been recommended to ensure that the integrity of the current existing natural features are protected and/or enhanced, and furthermore that their functions are not negatively impacted during or following construction.

A wetland was identified in the northeast corner of the Site. Development within 30 m of an unevaluated wetland may be permitted if it is determined to there will be no negative impact to the hydrologic function of the feature. The wetland identified on Site is limited in terms of its ecological function. The wetland area is relatively small (approximately 0.05 ha), has been historically altered and a hickenbottom structure was observed within the feature. The wetland appears to be fed primarily by stormwater drainage from surrounding developed areas. The current Draft Plan proposes integrating this feature into the future Stormwater Management Block, maintaining its hydrologic function. As such, the proposed development is not expected to result in adverse hydrologic impacts to the wetland and no additional avoidance measures or setbacks are recommended.

Fish habitat is present on and adjacent to the Site, in Whiskey Creek. A 30 m minimum vegetation protection zone (VPZ) is recommended to provide protection for this feature. The 30 m VPZ is considered sufficient to protect

the existing form and function of the feature provided that the area be maintained as existing forest cover and be allowed to naturally self-sustain (i.e., a buffer area where no vegetation removals or grading is allowed). Indirect impacts to fish habitat, including potential for changes to water quality, will be mitigated through appropriate Erosion and Sediment Control (ESC) measures and stormwater management

Three naturally occurring Butternut trees were documented on adjacent lands, approximately 3 m from the fence along the eastern property boundary. As these trees are off site, the proposed development does not require removals. Given that these trees are located on the neighbouring property, the proposed development does not require that they be removed; however, some level of disturbance within the 50 m buffers is anticipated. As such, the potential for harm to these trees can not be ruled out. A Notice of Butternut Impact form, accompanied by compensation planting/tending plans that meet regulatory requirements, are currently being prepared to address potential harm as a result of future on-site activities (e.g., grading), as per rules in Section 23.7 of O.Reg 242/08 under the ESA. This documentation will be submitted to MECP for review, prior to site alteration.

Further details regarding the features and mitigation measures are described in detail within the study.

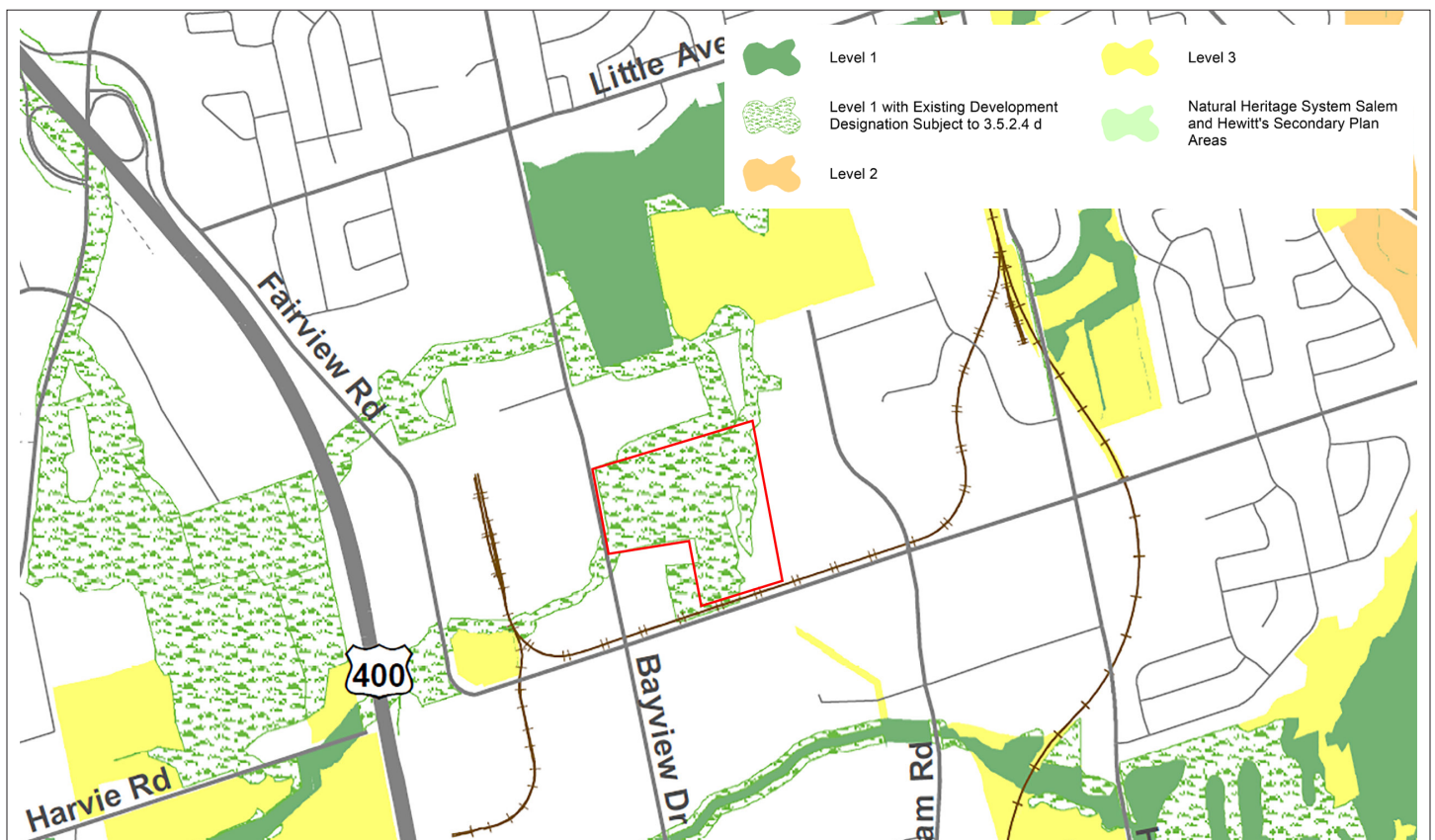


Figure 12. Official Plan - Schedule H Natural Heritage Resources

Land Use Policies — Industrial

Section 4.4 provides policies to guide development in the 'Industrial' designation. The goal of these policies and standards pertaining to the development of industrial land are sufficiently flexible to accommodate the changing and diverse needs to industry while ensuring that industrial development occurs in an environmentally sensitive manner.

There are five categories of industrial land use with the Plan, and the one which pertains to this application is General Industrial.

As noted, the subject lands are currently designated 'General Industrial' as per Schedule 'A' Land Use in the Official Plan (see figure 13). To facilitate the proposed development, a Draft Plan of Subdivision is required for the lands, to permit the 31 industrial lots, public Street 'A', as well as Block No.32 & Block No. 33.

A general policy of industrial land uses is to preserve lands adjacent to existing major highway interchanges and rail yards for manufacturing, warehousing and associated retail, office and ancillary facilities.

The policy (Section 4.4.2.1 (c) and (d) and (j)) also notes that lands within the vicinity of existing major highway interchanges and rail yards will be preserved for manufacturing, warehousing and associated retail, office and ancillary facilities, and that there approval will be subject to the availability of required urban services including municipal sewer and water, adequate vehicular access, off-street parking and loading facilities. Furthermore, adequate setbacks and landscaping of all industrial developments are to be provided when they abut open space, institutional and residential areas.

The subject lands shall not have a negative impact on adjacent uses, as adjacent uses are industrial. In addition, the intent for all future development within each lot or block would be to comply with the policy noted above and provide adequate setbacks for Industrial lots which are being proposed to comply with the existing applicable zoning.

General Industrial

The lands are currently designated 'General Industrial'. The predominant use within the 'General Industrial' designation shall be for manufacturing, processing, servicing, storage of goods and raw materials, industrial warehousing and similar such uses. Commercial uses which serve the industrial area, such as a restaurant as part of a multi-tenant building, may be permitted. Open storage activities on any lands shall be permitted subject to the appropriate landscaping and screening. Therefore the proposed application conforms with the 'General Industrial' designation.

While the subject site is located adjacent to a rail line, which may be utilized by future industrial lot owners, the site also provides natural buffering to the adjacent lands to the north and west. The existing watercourse and related buffers provide an increased setback and buffer to lands. There is also an existing open space landscape for the majority of the sites east flank. The site is also surrounded by compatible industrial uses, and has significant frontage on Bayview Drive and Big Bay Point Road, which will also act as separation buffers to surrounding uses. To the south, the rail line provides an increased separation from Big Bay Point Road.

The subject site lends itself to natural buffering, while proposing additional buffering where required. Through future Site Plan applications on each lot, the layout and detailed design of each lot within the Draft Plan of Subdivision will be further refined with landscaping and buffering measures through the site plan application process.

Land Use Policies — Environmental Protection Areas

Although not designated as such, a portion of the subject lands will be protected in a fashion similar to an 'Environmental Protection Area', which protects, preserves and enhances land with environmentally significant natural features and ecological functions and to maintain and improve the diversity of natural features or ecological functions for which an area is identified.

The intent of Environmental Protection Areas, and Block No. 33, is primarily for the preservation and conservation in the lands in natural state. Such uses as passive outdoor recreation, forestry, and wildlife management may be permitted where appropriate. No building or structures are proposed within Block No. 33.

Servicing and Transportation

Section 5.0 provides policies to help ensure that development can be appropriately serviced by municipal water, sanitary, and stormwater systems, and can be integrated within the existing transportation system. The proposed development will extend appropriate municipal services to each lot and block within the proposed subdivision, via Big Bay Point Road, through an 8.0 metre service easement. The subdivision will be serviced by the City's Waste Management program.

A stormwater management block has been provided to serve the entire subdivision. It is identified as Block No. 32 on the draft plan of subdivision. A Functional Servicing and Preliminary Stormwater Management Report, prepared by Pearson Engineering, dated August 2021, has been prepared and submitted in support of the proposal applications.

A new Street 'A' is proposed which provides access to the lots, and provides access for waste management and emergency vehicles. The road will be designed to industrial standards as a 20m right-of-way and will only provide access to the parcels which abut the road. The street will be built to a municipal cross section standard providing sidewalks within the right-of-way.

A 4.5 metre secondary emergency access point is provided at the southwest corner of the site and utilizes the existing railway crossing. It should be noted that the access along Bayview Drive has been designed to be larger road width to permit emergency services. Subject to review by Emergency Fire Services, the 4.5m secondary emergency access point is intended to be eliminated to facilitate more efficient use of the lot (lot 1).

As detailed in the Traffic Impact Study prepared by GHD dated August 2021, the overall impact of the proposed development is minor and will not adversely impact the operation of intersections Big Bay Point Road, Bayview Drive, Fairview Road, and Little Avenue. The proposed configuration and location of the site access to be acceptable, with no expected operational or safety concerns for Bayview Drive.

The proposed development lies in close proximity to the City of Barrie's Transit Routes 1 and 3.

Parkland Dedication Requirements

Development in the Industrial or Commercial land use designations, are required to provide two percent land dedication for park purposes or the equivalent cash-in-lieu in accordance with the Planning Act. Parkland dedications shall not include hazard lands, conservation areas or environmentally significant or sensitive areas, or requisite environmental buffer areas. Lands utilized for drainage purposes or stormwater management are not acceptable as part of the parkland dedication.

In calculating the parkland requirement, the Environmental Protection lands are to be excluded from the calculation. Parkland contributions will be in the form of cash-in-lieu for this development.

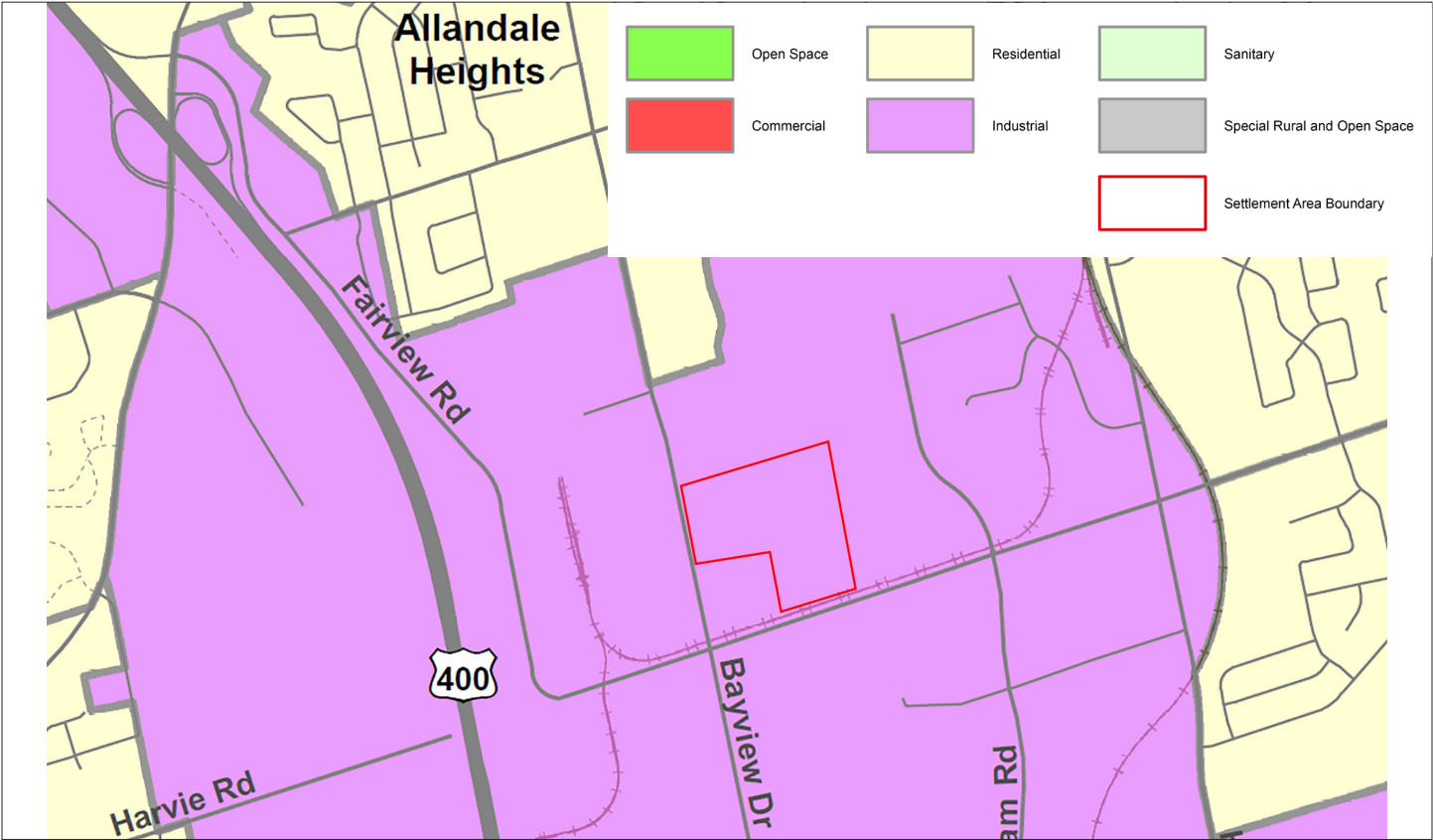


Figure 13. Official Plan Schedule B - Planning Areas

Urban Design Guidelines

Section 6.5 provides the City's Urban Design Guidelines, which provide a framework for the development and maintenance of a healthy, safe, convenient, efficient and aesthetically pleasing urban environment. At this time, end users are unknown, applying urban design guidelines to specific sites and their layouts is not possible, however the guidelines have been reviewed through the layout of the subdivision plan and all future lots shall conform with the guidelines when completing the site plan process.

Building and Siting (6.5.2.2 a.)

Buildings are to be designed to complement and contribute to a desirable community character in terms of massing and conceptual design. The design of roof's should screen mechanical equipment from public view and contribute to an attractive streetscape. Large exposed blank wall should be avoided, and if required, screened through landscaping. Building entrances should be well-defined and accessible to pedestrians and persons with disabilities. Pedestrian links should be designed to promote the safety of the user.

Parking Areas (6.5.2.2 b.)

Linking parking areas, driveways and access points should be encouraged, and mutual entrances will be encouraged for future development. Parking areas shall be divided by landscape to soften the visual impact through berming and planting. Major parking, loading and delivery areas, as well as garbage enclosures should be confined to the rear of future building development.

Landscaping (6.5.2.2 c.)

Landscaping planting strips are to be provided for future development along the street frontage and contain planting materials and street furniture consistent with any themes established by the municipality. Landscaping should seek to utilize native vegetation, and water conservation practices wherever feasible.

Environmental Features (6.5.2.2 d.)

Wherever possible the protection of treed areas, hedgerows and other natural areas shall be incorporated into the design, and the planting of new trees shall be encouraged. Development adjacent to Block No. 33 should be designed to incorporate the natural features and provide for their long term protection, subject to the results of an EIS.

Signage (6.5.2.2 e.)

Signs shall complement the architectural design and materials of the buildings and be located in accordance with the Sign By-law.

Utilities (6.5.2.2 f.)

Consideration shall be given to the location of utilities within the public rights-of-way as well as on private property within appropriate easements. Utilities shall be clustered or grouped where possible to minimize visual impact. The City encourages utility providers to consider innovative methods of containing utility services on or within streetscape features such as gateways, lamp posts, and transit shelters.

Energy Efficient Urban Design (6.5.2.2 g.)

Energy efficiency shall be encouraged through community, site, and building design measures that use energy efficient building materials, energy conserving landscaping, building orientation that uses shade and sunlight to advantage, panels for solar energy, appropriate lighting, "green" roofs, and other methods. Energy efficiency is promoted through the development of a compact urban form that encourages the use of transit, cycling, and walking, a mix of housing and employment uses to shorten commuting trips, and focusing major developments on transit routes.

Based on the review above, the proposed application conforms with the intent and policies of the Official Plan.

CONCLUSION

5

PAGE INTENTIONALLY LEFT BLANK

5.1 Conclusion

The proposed Draft Plan of Subdivision Application proposes a new premiere industrial subdivision including 31 industrial lots, 1 Environmental Protection Block, and 1 Stormwater Management Block. These blocks range in size and meet the required performance standards for the General Industrial Zoning. A new 20m municipal right-of-way will be developed to service the subdivision extending east from Bayview Drive at the southern most section of the properties Bayview Drive frontage. The new right-of-way will be built to industrial standards and extends east into the site creating a loop within the interior of the subject property.

The lands are located in an industrial / employment area, within the City of Barrie. The subdivision would facilitate the creation of lots ready for industrial use, thereby creating employment opportunities in the City.

Based on the results of the technical analysis and on an evaluation of the applicable planning policy, the proposed applications:

- are consistent with the applicable policies of the Provincial Policy Statement 2020;
- conform with the applicable policies of the Growth Plan for the Greater Golden Horseshoe 2020;
- conform with the applicable policies of the Lake Simcoe Protection Plan;
- conform with the applicable policies of the City or Barrie Official Plan (Office consolidation - January 2018);
- comply with City of Barrie Zoning By-law (Office consolidation - January 2020);
- do not negatively impact significant / key natural heritage and key hydrologic features, functions and areas, or the Provincial Natural Heritage System;
- do not contain, adjoin or impact significant cultural heritage / archaeological resources;
- do not contain, adjoin or are impacted by any natural or human-made hazards;
- create much needed opportunities for employment in an area designated for such use; and,
- represents good planning.

Respectfully submitted,
Innovative Planning Solutions



Darren Vella, MCIP, RPP
President & Director of Planning



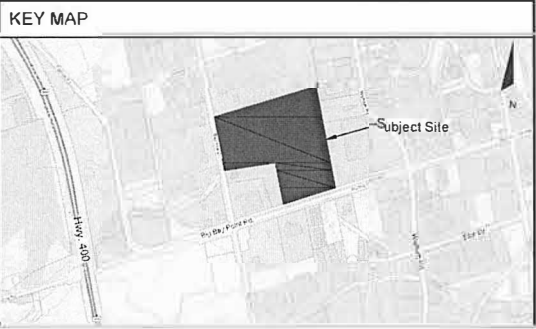
Kyle Galvin, MCIP, RPP
Senior Planner



IPS

INNOVATIVE PLANNING SOLUTIONS

PLANNERS • PROJECT MANAGERS • LAND DEVELOPMENT



DRAFT PLAN OF SUBDIVISION

Part of West Half of Lot 9, Concession 13,
Geographic Township of Innisfil,
City of Barrie,
County of Simcoe
Scale 1:1,000

LEGEND

- SUBJECT LANDS
- PHASE LINE

OWNER'S CERTIFICATE
I HEREBY AUTHORIZE INNOVATIVE PLANNING SOLUTIONS TO PREPARE THIS DRAFT PLAN OF SUBDIVISION AND SUBMIT THIS DRAFT PLAN OF SUBDIVISION FOR APPROVAL.
Aug. 24, 2021
DATE
TONLU HOLDINGS LIMITED


SURVEYOR'S CERTIFICATE
I CERTIFY THAT THE BOUNDARIES OF THE LAND TO BE SUBDIVIDED AND THEIR RELATIONSHIP TO ADJACENT LANDS ARE ACCURATELY AND CORRECTLY SHOWN.
AUG. 24, 2021
DATE
C. WAHBA SURVEYING LTD.

ADDITIONAL INFORMATION REQUIRED UNDER SECTION 51(17) OF THE PLANNING ACT

- a) SHOWN ON PLAN
- b) SHOWN ON PLAN
- c) SEE KEY PLAN
- d) RESIDENTIAL
- e) SHOWN ON PLAN
- f) SHOWN ON PLAN
- g) SHOWN ON PLAN
- h) MUNICIPAL WATER
- i) SAND, SILT GLACIAL TILL
- j) SHOWN ON PLAN
- k) MUNICIPAL WATER & SEWAGE
- l) NONE

LAND USE STATISTICS			
LAND USE	LOT No.	BLOCK No.	AREA (ha)
Industrial Lots	1 - 31	32	9.7
SWM		32	1.7
Environmental Protection		33	2.5
Streets			1.7
TOTAL:	31	33	15.6

General Industrial (GI) Zone		
Provisions	Provided	Required
Lot Area (min.)	700.0m ²	2,219.6m ²
Lot Frontage (min.)	15.0m	35.1m
Front Yard (min.)	7.0m	> 7.0m
Side Yard (min.)	3.0m	> 3.0m
Side Yard Adjoining Street (min.)	7.0m	> 7.0m
Rear Yard (min.)	7.0m	> 7.0m
Lot Coverage (max.)	60%	< 60%
Building Height (max.)	-	-

**INNOVATIVE PLANNING SOLUTIONS**
PLANNERS • PROJECT MANAGERS • LAND DEVELOPERS
847 WILHAM ROAD, UNIT 5A, BARRIE, ON L4N 6B7
Tel: 705-812-3261 Fax: 705-812-3428 Email: info@ipsconsulting.com www.ipsconsulting.com

Date:	August 9, 2021	Drawn By:	A.S.
File:	20-915	Checked:	D.V.